



Planning & Zoning Department

## Planning & Zoning Commission Meeting

July 12, 2016

### Staff Report – Public Hearing # 5

**To:** Planning & Zoning Commission

**Applicant:** Idaho Department of Health & Welfare / Doug Russell representing

**File No's:** CMA 029-2016, ZMA 016-2016, PUD 002-2016

**Prepared By:** Karla Nelson

**Date:** July 6, 2016

**Requested Actions:** 1) Comprehensive Plan Future Land Use Map Amendment from Public and Parks to Community Mixed Use 2) Rezone from AG (Agricultural) to GB1–PUD (Gateway Business 1 as a Planned Unit Development) at 1660 11<sup>th</sup> Ave North (Approximately 600 acres of land located in Sections 11,12,13, and 14, T3N, R2W, BM, Canyon County, Idaho)

**Purpose:** To provide for a mixed use center consisting of commercial, office, retail, civic, single family residential, multi-family residential, retirement homes and golf course/ open space.

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### BACKGROUND INFORMATION

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**History:** The site was utilized as a care facility for people with physical and learning disabilities starting in the early 20<sup>th</sup> Century. In the 1980's the State of Idaho explored various possibilities to use some of the land for different purposes resulting in Centennial (1985) and Ridgecrest (1994) Golf Courses, the Department of Labor Job Corps campus (1995) and a juvenile corrections facility (1996).

The city of Nampa originally had a 25-year lease for Centennial and a 20-year lease for Ridgecrest golf courses. In December of 2014 a five year extension of both leases was approved. The lease extensions will expire on December 31, 2019. However, there is a clause that the State of Idaho or the city can terminate the leases early with a 90 day notice.

**Conceptual Master Plan:** In July of 2013, Idaho Department of Health and Welfare completed a conceptual master plan for the Southwest Idaho Treatment Center (SWITC) site. The plan envisions a mix of uses that promote pedestrian accessibility throughout the development, connectivity to adjacent uses, and high quality architecture, landscaping and streetscape design. Design guidelines cover the overall site development, landscaping, and architecture. The application before Nampa Planning and Zoning Commission and City Council does not approve the conceptual master plan but it does consider a zoning and comprehensive plan map change that would allow for uses envisioned in the master plan.

The SWITC conceptual master plan includes an economic and fiscal impact analysis. If the land is developed as envisioned by the master plan the estimated total taxable value would be \$600 Million. Estimated annual tax revenue at full build-out per taxing entity is as follows:

- City of Nampa - \$6,954,600
- Nampa School District - \$2,761,100
- Canyon County - \$3,427,700
- Nampa Highway District - \$908,600
- Vallivue School District - \$3,274,300
- College of Western Idaho - \$114,400

### **Environmental**

The Department of Environmental Quality keeps a database of historic landfill sites. The database includes a record for Canyon County Section 12, Subsection SW ¼; SW ¼, Township 03N; Range 02W, which is roughly the model airplane runway location. This area was a demolition disposal facility that is now closed. According to the Department of Environmental Quality, buried waste might be encountered during future excavation activities. Waste, if found, will need to be properly characterized and handled for proper disposal.

### **Public Utilities:**

**Water:** Domestic waterlines are onsite, 12" mains on Ridgecrest Drive and 11<sup>th</sup> Avenue North. Future developers could connect into existing services, but a network of mainlines would need to be installed through the development, along with some pressure reducing valves.

**Sewer:** An 8" gravity sewer main serves a portion of 11<sup>th</sup> Avenue North. Future developers will likely need to make off-site improvements to handle additional capacity needs, this is currently under evaluation.

**Irrigation:** There is currently a private irrigation system that serves the site that could remain private or be upgraded to public standards.

Needed public utility improvements will be paid for by the owner or developer and stipulated in the Development Agreement.

**Emergency Services:** All available.

**Transportation:** The property is accessed from 11<sup>th</sup> Avenue North Ext. and Ridgecrest Drive. Full build out of the master plan will require improved site access and internal connectivity. The 2013 conceptual master plan envisions a new interchange along I84. After discussing this option with the Idaho Transportation Department, the applicant has since removed the interchange from the master plan. Alternatively they are now proposing an overpass at 39<sup>th</sup> street and an overpass on the western end of the development down to E. Karcher Road. Transportation improvements will be paid for by the developer.

**Correspondence and Public Input:** To date there have been a few phone calls and an email from citizens interested in preserving Ridgecrest and Centennial Golf Courses. During the summer of 2015 approximately 50 people showed up to a community open house regarding the Northeast Nampa Specific Area Plan. Nearly all of the attendants were concerned about potential changes to the golf courses.

**Status of Applicant:** Owner / Owner Representative

**Location:** North of I84 at 1660 11<sup>th</sup> Avenue North including Centennial and Ridgecrest Golf Courses.

**Size of Area:** Approximately 600 acres of land located in Sections 11, 12, 13, and 14, T3N, R2W, BM, Canyon County, Idaho

**Current Comprehensive Plan Land Use Designation:** Parks and Public

**Proposed Comprehensive Plan Land Use Designation:** Community Mixed Use

**Existing Zoning:** AG (Agricultural)

**Proposed Zoning:** GB1- PUD (Gateway Business 1 as a Planned Unit Development)

**Surrounding Land Use and Zoning:**

North- Primarily residential, RS 6

South- Primarily Industrial, IP and IL, some commercial, BC

East- Commercial, GB 1

West – Primarily Industrial (IP & IL) some commercial, BC

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**APPLICABLE REGULATIONS**

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**Comprehensive Plan Future Land Use Map Amendment**

According to state law, changes to the Comprehensive Plan Future Land Use Map may be recommended by the Planning and Zoning Commission at any time.

The decision to approve or deny a comprehensive plan map amendment/ change is purely subjective. However, the City attorney has advised that the Planning and Zoning Commission and City Council provide some rational basis for the proposed change. Rationale could include substantial changes to the surrounding area and/or errors in the original plan.

The “Community Mixed Use” designation is recommended for activity centers that include commercial, office and residential uses. These areas include an interconnected circulation system that is convenient for automobiles, pedestrians and transit. Well planned mixed-use developments are encouraged by Nampa’s comprehensive plan.

**Rezone**

In regard to the corresponding rezone request there are several criteria to consider. Rezones must be in harmony with the comprehensive plan; be reasonably compatible with existing, adjoining property uses; establish an area of zoning the same as or compatible with immediately adjoining districts; not create a “spot” zone; and be in the interest of the public and reasonably necessary.

The existing AG (Agricultural) district is meant to preserve the economic and social value of agricultural operations. Within the city it also serves as a transition between rural and urban. (Ord. 2140)

The purpose of the proposed GB1 (Gateway Business) district is to encourage the consistent development of areas surrounding community gateways or entryways. GB1 is intended for mixed use, primarily commercial development allowing for a variety of highest and best land use alternatives with flexible development standards. High quality architecture, landscaping and site planning standards are encouraged. (Ord. 3450, 5-23-2005)

**Planned Unit Development**

The proposed Planned Unit Development (PUD) would allow greater flexibility and more creative design for the development. All uses allowed within the underlying land use district are permitted within a PUD and up to 20% of the gross land area may be directed to uses not otherwise allowed, in this case residential. There are several PUD approval criteria for the commission to consider. The development should be consistent with the PUD zoning ordinance chapter (10-26),

the proposal should conform to the comprehensive plan, the PUD should be compatible with the surrounding area and the improved design of the development should have a beneficial effect which would not be achieved under standard district regulations.

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## STAFF FINDINGS

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Regarding the requested **Comprehensive Plan Map Amendment** from Parks and Public to Community Mixed Use, the Planning & Zoning Commission must determine a justification for approval or denial. As previously stated, a plan may be amended for any logical or reasonable basis.

It should be noted that there are no Community Mixed Use designations adjacent to the subject property. The closest such designation is about 500' to the north on the west side of Idaho Center Boulevard. However, the subject property is separated from surrounding land uses by the railroad corridor, 184 and steep slopes. These barriers provide some buffer to surrounding land uses.

Although the future land use map does not currently designate community mixed use for this area the Comprehensive Plan does encourage mixed use developments. Smart growth principles advocated for in Nampa's Comprehensive Plan support mixed land uses as a critical component of achieving better places to live. By allowing residential, commercial, office and recreational uses in close proximity, alternatives to driving, such as walking or biking, become viable. Mixed use developments tend to provide a larger variety of housing options, shorter travel times between work and home environments, a strong community atmosphere, and pedestrian friendly lifestyles.

A desire to see more mixed use developments in Nampa along with the master plan developed by the Idaho Department of Health and Welfare could all justify a Comprehensive Plan Map Amendment to Community Mixed Use.

Under Section 10-2-3 regarding rezones, in order to approve of the proposed **Rezone from AG (Agricultural) to GB1 (Gateway Business)** the Planning & Zoning Commission must find the following:

1. The proposed map amendment (rezone) would be in harmony with the city's currently adopted comprehensive plan and comprehensive plan future land use map (or as recommended for amendment);  
*This requirement could be met if the Comprehensive Plan map amendment is approved.*
2. The proposed map amendment (rezone) would provide for a proposed use or set of uses that would be at least reasonably compatible with existing, adjoining property uses;  
*The site is separated from surrounding land uses by the railroad corridor, 184 and steep slopes. The property is bordered by commercial uses to the east, industrial to the west and the master plan places residential and golf course uses adjacent to existing residential to the north.*
3. The proposed map amendment (rezone) would make a change on the land use map of the city which would establish an area of zoning the same as or compatible with immediately adjoining districts;  
*The proposed GB1 zoning is currently designated for the area to the east. The conceptual site plan also congregates residential and golf course uses on the northern section which is bordered by a residential subdivision. The conceptual land use layout generally*

*matches surrounding lands and interior buffers are designed to ease transitions between land uses.*

4. The proposed map amendment (rezone) would not create a "spot" zone (having a section of one kind of zoning surrounded by another) having no supportive basis per the adopted comprehensive land use map so as to only serve to benefit the applicant;

*The rezone is adjacent to GB1 zoning on the east end and therefore would not create a spot zone.*

5. The proposed map amendment (rezone) would be in the interest of the public and reasonably necessary.

*The proposed rezone would allow for the proposed SWITC master plan development which if fully realized is estimated to generate 6.9 million in annual tax revenues for the City of Nampa. The envisioned development would also create a quality mixed use development that would become a desirable amenity for Nampa.*

*Existing uses including the golf courses also provide a public good. However, the city cannot require the state to use the land for golf. Any use permitted by the zoning district is allowed. To ensure that the golf courses are preserved, the city would have to purchase the land from the Department of Health and Welfare.*

The GB1 zoning district does not allow residential uses and some of the setback requirements would not support the proposed conceptual master plan. Therefore a Planned Unit Development (PUD) is being considered along with the rezone request. Under Section 10-26-7 regarding **Planned Unit Developments**, approval criteria include:

1. The proposed development is consistent in all respect with the spirit and intent of the PUD Chapter.

*The proposal is generally compliant with Nampa's Zoning Ordinance PUD chapter (10-26) and allows greater flexibility and more creative design as intended. Future development plans will need to comply with specific standards in this chapter.*

2. The proposal should conform to Nampa's comprehensive plan.

*The proposal contributes to Nampa 2035 Comprehensive Plan by helping to achieve two goals. Chapter 5, Goal 12: Encourage the development of compact, mixed-use neighborhoods, districts and centers; and Chapter 5, Goal 21: Use Smart Growth principles as guidelines for development, wherever possible. The proposal will also be in conformance with the comprehensive plan if the comprehensive plan map amendment is approved.*

3. The PUD will be compatible with the surrounding area.

*The rail corridor, I84, and slopes largely separate the site from surrounding land uses, these barriers help to mitigate impacts from uses allowed on the site. Furthermore, the conceptual site plan includes residential and golf course uses in the northern section, the only section bordered by residential subdivisions. These subdivisions, Sunset Oaks and the Greens at Ridgecrest are separated from the site by the railroad corridor.*

4. The benefits and improved design of the development will have a beneficial effect which would not be achieved under standard district regulations.

*The proposed mix of land uses and setbacks would not be allowed in any of Nampa's existing zoning districts. The PUD process is the only way to achieve this type of mixed use development, supported by our comprehensive plan and smart growth principles. Most residential land uses are not permitted in the GB1 district, neither is zero foot street side setbacks proposed for the retail/ mixed use areas. Including residential uses in close*

*proximity to retail, commercial, office and employment centers allows for more compact development and greater transportation options. Zero foot setbacks are desirable for lively pedestrian oriented environments.*

5. Project design elements including landscaping, siting, architecture, street sections, easements, maintenance, ownership, should all be considered.

*The conceptual master plan includes design guidelines that if followed will create an aesthetically pleasing environment that will be functional for property owners, businesses, residents and visitors to the planned area. Proposed design guidelines are attached.*

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## CONDITIONS OF APPROVAL

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If the Planning and Zoning Commission determines that the proposed comprehensive plan map amendment and rezone with a PUD is appropriate for the location, certain **conditions of approval** are recommended. Conditions of approval for the rezone are included in the attached draft Development Agreement. The following are recommended for the PUD permit.

1. The project shall be developed in substantial conformance with the site plan (attached). The owner shall have limited flexibility to develop the Property to meet market conditions.
2. Design guidelines 03.2 through 03.6 outlined in the Southwest Idaho Treatment Center Conceptual Master Plan Final Report dated July 2013 shall be followed with substantial conformance. The owner shall have limited flexibility to develop the Property to meet market conditions.
3. Up to 20% of the gross land area may be directed to uses not typically allowed in the GB1 district, based on the proposal, the use exceptions will be residential.
4. Individual uses and structures in the PUD need not comply with the specific regulations of the underlying GB1 district provided the requirements in (10-26-4 and 10-26-6) are adhered to, specifically:
  - a. Fire Regulations: Where two walls oppose each other minimum separation shall be required by city fire regulations.
  - b. Light and Air: Building spacing may be reduced where there are no windows or very small window area and where rooms have adequate provisions for light and air from another direction.
  - c. Building Separation: Any detached structure shall be set at least six feet apart.
  - d. Parking Space Clearance: Any garages, carports or parking pads shall be no closer to the drive, street or alley which they access than twenty feet.
  - e. Access: Access to a public street is assured to each and every building lot/ parcel by recorded easement.
  - f. Setback: At least five feet is maintained between any detached structure and a side or rear building lot property line.
  - g. Height of Buildings: Building heights, if increased beyond that normally allowed in the zone in which the PUD is located are not increased by more than two stories over and above the height normally allowed and this only when the PUD does not abut an existing single-family residential subdivision on the side(s) of the PUD where the height increase is desired.
  - h. Reduced Property Area: For a structure it is sufficient to fully contain that structure on a single lot/ parcel.
  - i. Zero Lot Line Structure Placement(s): Zero lot line construction is allowed provided the following requirements are met:
    - i. In the case of common wall construction all applicable city, state and federal building regulations shall be complied with/
    - ii. Sites shall be selected to avoid drainage problems since it becomes more difficult for each lot to drain on its own with one side yard eliminated.

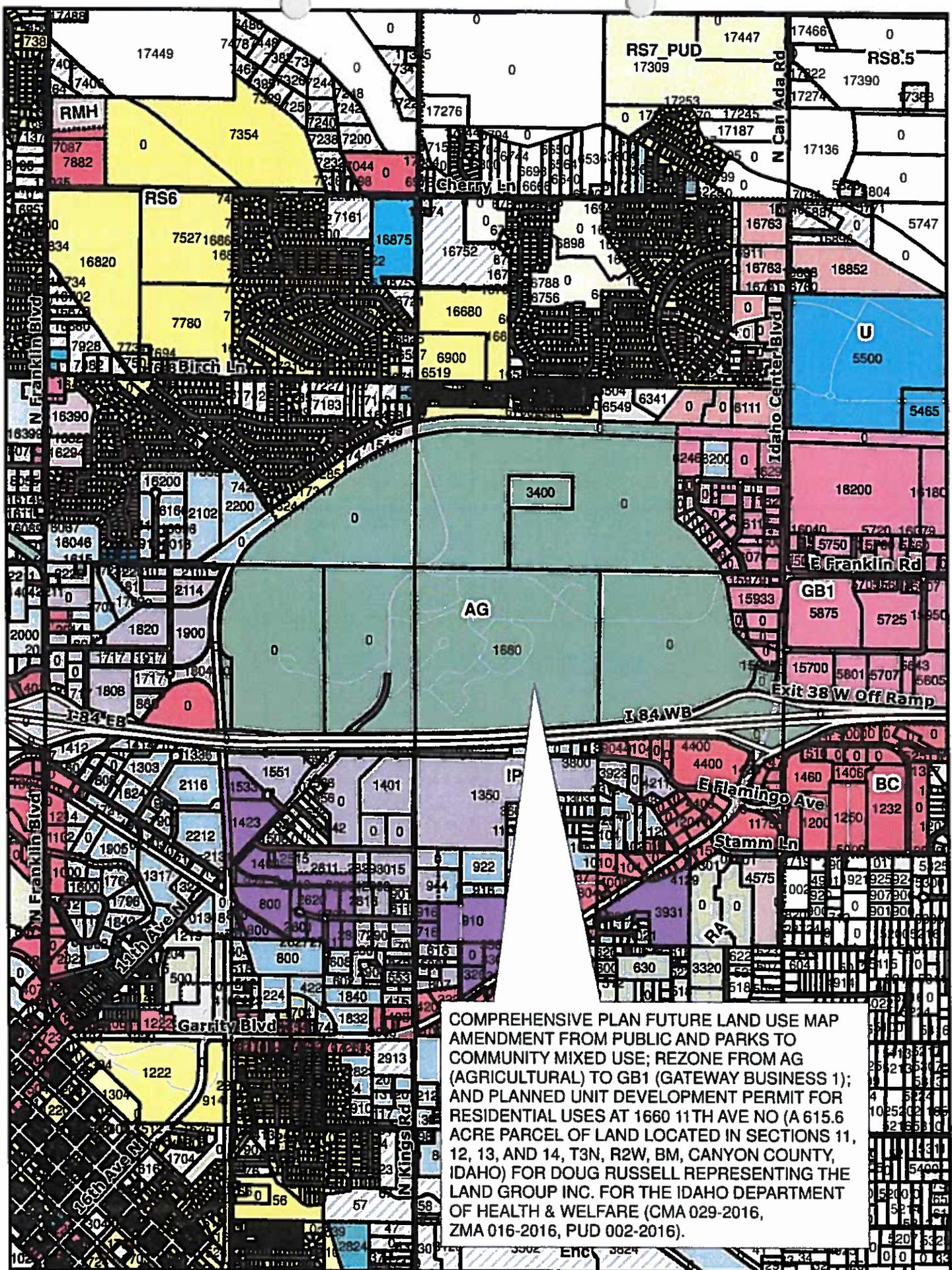
- iii. Adjoining lot shall provide a five foot maintenance easement on the zero lot line side.
5. Engineering Division conditions outlined in the Development Agreement conditions of approval numbers 4 through 13.

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## ATTACHMENTS

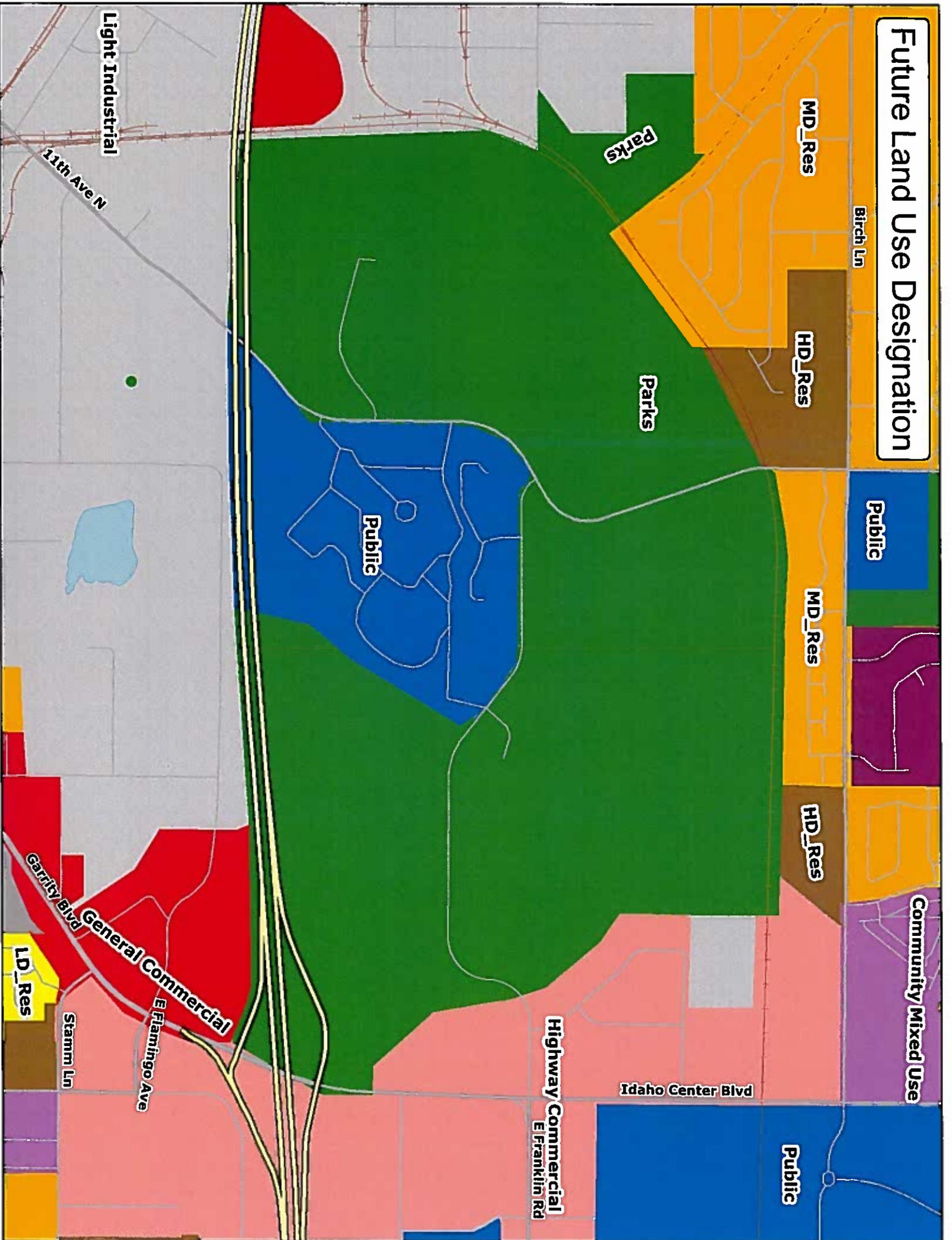
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- Location, zoning, comprehensive plan future land use maps
- Southwest Idaho Treatment Center site plan
- Draft Development Agreement
  - Southwest Idaho Treatment Center Conceptual Master Plan Design Guidelines
- Application(s) and letter
- Agency and neighboring property owner correspondence



COMPREHENSIVE PLAN FUTURE LAND USE MAP  
 AMENDMENT FROM PUBLIC AND PARKS TO  
 COMMUNITY MIXED USE; REZONE FROM AG  
 (AGRICULTURAL) TO GB1 (GATEWAY BUSINESS 1);  
 AND PLANNED UNIT DEVELOPMENT PERMIT FOR  
 RESIDENTIAL USES AT 1660 11TH AVE NO (A 615.6  
 ACRE PARCEL OF LAND LOCATED IN SECTIONS 11,  
 12, 13, AND 14, T3N, R2W, BM, CANYON COUNTY,  
 IDAHO) FOR DOUG RUSSELL REPRESENTING THE  
 LAND GROUP INC. FOR THE IDAHO DEPARTMENT  
 OF HEALTH & WELFARE (CMA 029-2016,  
 ZMA 016-2016, PUD 002-2016).

# Future Land Use Designation



# SWITC

Southwest Idaho Treatment Center | Nampa, ID | Idaho Department of Health & Welfare | June 14, 2016



THE LAND GROUP



## DEVELOPMENT AGREEMENT

THIS DEVELOPMENT AGREEMENT (this “**Agreement**”), is made and entered into this \_\_\_\_\_ day of \_\_\_\_\_, 2016 (the “**Effective Date**”), by and between the City of Nampa, a municipal corporation, hereinafter referred to as the “**City**,” and:  
\_\_\_\_\_, hereinafter referred to as “**Owner**.”

### RECITALS

- A. Owner is the owner of approximately 615 acres of real property legally described in Exhibit “A” attached hereto and made a part hereof (the “**Property**”).
- B. Owner applied to the City on June 14, 2016 (the “**date of application**”) to amend the Comprehensive Plan Future Land Use Map from Parks and Public to Community Mixed Use and to Rezone the property from AG (Agricultural) to GB1 (Gateway Business1) as a Planned Unit Development PUD that would allow up to 20% residential uses in anticipation of the development and construction of a mixed-use project comprising commercial office, retail, restaurants, multi-family residential, single family residential, open space for soccer and golf, retirement community, 3 hotels, a transit center and job corps (the “**Project**”).
- C. City, pursuant to Section 10-2-5, Nampa City Code, and Idaho Code Section 67-6511A, has the authority to rezone the Property and amend a Development Agreement for the purpose of allowing, by agreement, a specific development to proceed in a specific area and for specific purposes and/or uses that are appropriate in the area.
- D. City’s Planning and Zoning Commission and City’s City Council have held public hearings as prescribed by law with respect to the Comprehensive Plan Map Amendment, rezoning and development of the Property and this Agreement. City has approved the requested rezoning of the Property to GB1 Gateway Business 1 as a Planned Unit Development that allows up to 20% residential land use.

### AGREEMENT

**NOW THEREFORE**, in consideration of the above recitals, which are incorporated below, and of the mutual covenants and agreements herein contained, and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, the parties hereto agree as follows:

1. This Agreement shall not prevent the City, in subsequent actions applicable to the Property, from applying new ordinances and regulations of general application adopted by the City in the exercise of its police powers that do not conflict with the parties’ commitments applicable to the Property as set forth herein, or the zoning designation approved hereby as the Property has been deemed suitable for the uses allowed within said zoning designation with the stipulated modifications in the Conditions of Approval.

2. The Project shall be developed in substantial conformance with the conceptual master plan attached hereto as **Exhibit “B”** (the **“Conceptual Plan”**) and made a part hereof; provided, however, that Owner/Developer shall have limited flexibility to develop the Property to meet market conditions, and the only specific commitments concerning development of the Project which the Owner/Developer is making are set forth herein. Upon recordation of this Agreement, Owner/Developer shall have all approvals required from the City for development of the Project in general conformance with the Conceptual Plan.
3. This Agreement is intended to be supplemental to all other local, city, state and federal Code requirements, rules and regulations, and is established to help assure the compatibility of the resulting land use with the surrounding area. Provided, however, that to the extent this Agreement conflicts with any provision of the Nampa City Code, this Agreement shall prevail to the extent permitted by law.
4. The provisions and stipulations of this Agreement shall be binding on City, Owner/Developer, each subsequent owner of the Property or portion thereof, and each person acquiring an interest in the Property and are, in no particular order, as set forth in the conditions of approval attached hereto as **Exhibit “C”**, and by this reference incorporated herein.
5. This Agreement may be modified only by the written agreement of Owner/Developer and the City after complying with the notice and hearing procedures required under Idaho Code Section 67-6511A or Nampa City Code Section 10-2-5(D) or successor provisions.
6. The execution of this Agreement and the written commitments contained herein shall be deemed written consent to change the zoning of the Property to its prior designation upon failure of Owner/Developer to comply with the terms and conditions of this Agreement. Provided, however, that no such consent shall be deemed to have been given unless City provides written notice of any such failure and Owner/Developer or its successors and/or assigns fails to cure such failure as set forth below.
7. This Agreement and the commitments contained herein shall be terminated, and the zoning designation reversed, upon the failure of Owner/Developer, or each subsequent owner or each person acquiring an interest in the Property, to comply with the commitments contained herein within two (2) years after the Effective Date, and after the notice and hearing requirements of Idaho Code Section 67-6509 have been complied with by City. Exception: the failure to begin site development of all or a portion of a project proposed under this Agreement does not necessarily serve as impetus to allege that the commitments contained herein are not being fulfilled. Rather, commencement of site work and/or construction then left in abandon or failure to abide by the terms of this Agreement, as herein iterated, shall serve as impetus to consider termination of this Agreement and reversion of zoning. Provided, however, no such termination or reversal shall occur unless City provides written notice of Owner/Developer’s failure to comply with the terms and conditions of this Agreement to Owner/Developer and Owner/Developer fails to cure such failure within six (6) months of Owner/Developer’s receipt of such notice. The two (2) year period of time for compliance with commitments may be extended by City for good cause upon application for such extension by Owner/Developer, and after complying with the notice and hearing provisions of Idaho Code Section 67-6509.

8. Except as specifically set forth in this Agreement, the rules, regulations and official policies governing permitted uses of land, density, design, improvements and construction standards and specifications applicable to the Project and the Property shall be those rules, regulations and official policies in effect as of the date of rezone and comprehensive plan map amendment. Provided, however, that the applicable building codes for structures shall be the codes in effect when a complete application for a building permit is filed. Development impact fees, if imposed by ordinance, shall be payable as specified in said ordinance even if the effective date is after the date of this agreement.

9. It is intended by the parties that this Agreement shall be recorded on the Effective Date or as soon as practicable thereafter. The parties further intend that the provisions of this Agreement shall run with the Property and shall be binding upon City, Owner/Developer, each subsequent owner of the Property, and each other person or entity acquiring an interest in the Property.

10. If any term or provision of this Agreement, to any extent, shall be held invalid or unenforceable, the remaining terms and provisions herein shall not be effected thereby, but each such remaining term and provision shall be valid and enforced to the fullest extent permitted by law.

11. This Agreement sets forth all promises, inducements, agreements, conditions and understandings between Owner/Developer and City relative to the subject matter hereof. There are no promises, agreements, conditions or understandings, either oral or written, express or implied, between Owner/Developer and City, other than as are stated herein. Except as herein otherwise provided, no subsequent alteration, amendment, change or addition to this Agreement shall be binding upon the parties hereto unless reduced to writing and signed by the parties or their successors-in-interests or their assigns, and pursuant, with respect to the City, to a duly adopted ordinance or resolution of the City.

12. Should any litigation be commenced between the parties hereto concerning this Agreement, the prevailing party shall be entitled, in addition to any other relief as may be granted, to court costs and reasonable attorneys' fees as determined by a court of competent jurisdiction.

13. This Agreement may be executed in counterparts, each of which shall constitute an original, all of which together shall constitute one and the same Agreement.

14. In the event Owner/Developer, its successors, assigns or subsequent owners of the Property or any other person acquiring an interest in the Property, or in the event City, fail to faithfully and materially comply with all of the terms and conditions included in this Agreement, enforcement of this Agreement may be sought by either City or Owner/Developer or by any successor or successors in title or interest or by the assigns of the parties hereto, in an action at law or in equity in any court of competent jurisdiction.

a. A waiver by City of any default by Owner/Developer of any one or more of the covenants or conditions hereof shall apply solely to the breach waived and shall not bar any other rights or remedies of City or apply to any subsequent breach of any such or other covenants and conditions. A waiver by Owner/Developer of any default by City of any one or more of the covenants and conditions hereof shall apply solely to the breach

waived and shall not bar any other rights of remedies of Owner/Developer or apply to any subsequent breach of any such or other covenants and conditions.

b. Notwithstanding anything to the contrary herein, in the event of a material default of this Agreement, the parties agree that City and Owner/Developer shall have thirty (30) days after delivery of notice of such default to correct the same prior to the non-defaulting party's seeking of any remedy provided for herein; provided, however, that in the case of any such default which cannot with diligence be cured within such thirty (30) day period and thereafter shall prosecute the curing of same with diligence and continuity, then the time within which such may be cured shall be extended for such period as may be necessary to complete the curing of the same with diligence and continuity, but in any event not to exceed six (6) months; and provided further, however, no default by a subsequent owner of a portion of the Property shall constitute a default by Owner/Developer for the portion of the Property still owned by Owner/Developer.

c. In the event the performance of any obligation to be performed hereunder by either Owner/Developer or City is delayed for causes that are beyond the reasonable control of the party responsible for such performance, which shall include, without limitation, acts of civil disobedience, strikes or similar causes, the time for such performance shall be extended by the amount of time of such delay.

d. In addition to the remedies set forth above, in the event of a default by Owner/Developer, or any other party claiming an interest herein, City may withhold building permits for any remaining lots within the development until such time as the default is cured.

IN WITNESS WHEREOF, the parties hereto have hereunto set their hands on this day and year first above written.

CITY OF NAMPA

\_\_\_\_\_  
Robert L. Henry, Mayor

\_\_\_\_\_  
Attest: Debbie Bishop, City Clerk

OWNER/DEVELOPER(S)  
\_\_\_\_\_

STATE OF IDAHO )  
                          ) ss.  
County of Canyon )

On this \_\_\_ day of \_\_\_\_\_, in the year of 2016, before me \_\_\_\_\_, personally appeared Robert L. Henry, known or identified to me, to be the Mayor of the City of Nampa, whose name is subscribed to the within and foregoing instrument and acknowledged to me that he executed the same, and was so authorized to do so for and on behalf of said City of Nampa.



**EXHIBIT “A”**

**LEGAL DESCRIPTION OF THE PROPERTY**

**EXHIBIT “B”**  
**CONCEPTUAL PLAN**



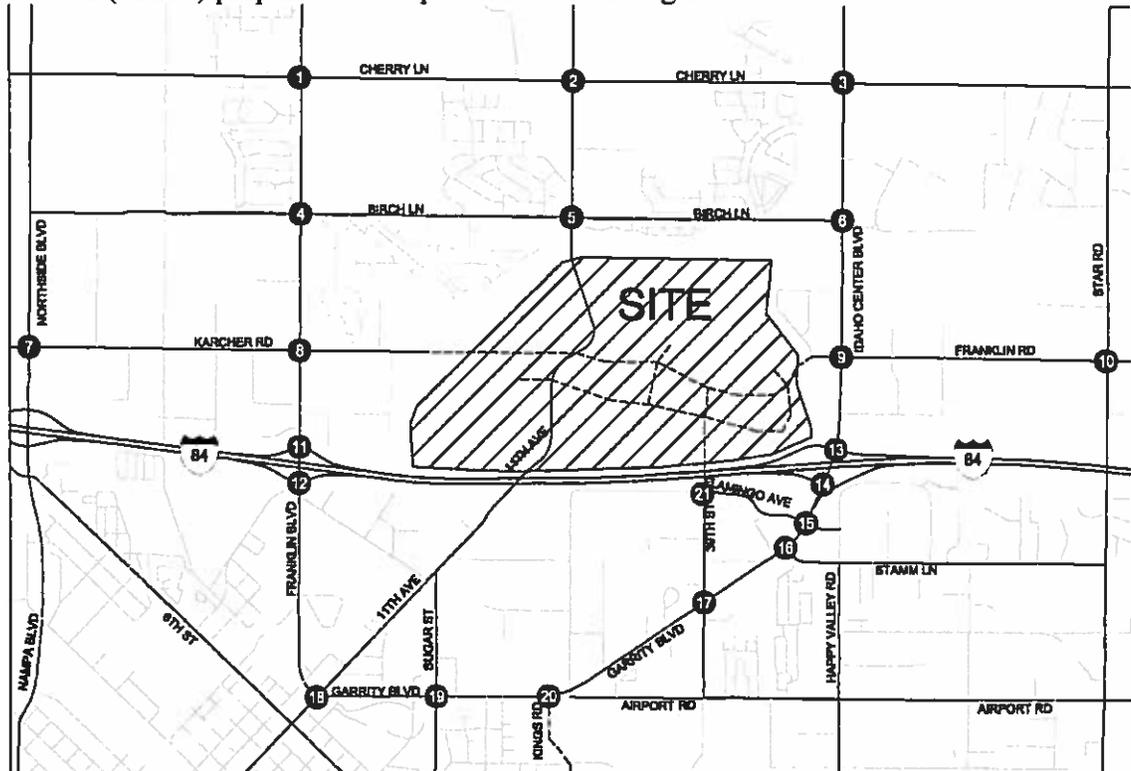
## EXHIBIT “C”

### CONDITIONS OF APPROVAL

1. The Project shall be developed in substantial conformance with the conceptual master plan attached hereto as **Exhibit “B”** (the **“Conceptual Plan”**) and made a part hereof; provided, however, that Owner/Developer shall have limited flexibility to develop the Property to meet market conditions.
2. Design guidelines outlined in the Southwest Idaho Treatment Center Conceptual Master Plan Final Report dated July 2013 section 03.2 through 03.6 attached as Exhibit “D” shall be followed with substantial conformance provided, however, that Owner/Developer shall have limited flexibility to develop the Property to meet market conditions.
3. Up to 20% of the gross land area may be directed to residential uses which are typically not allowed in the GB1 district.
4. This is a long term development project that will be phased and implemented over an extended period of time. All land divisions of any size or kind shall be required to go through the City’s preliminary and final plat process even if the size of the parcels might otherwise qualify for an exemption from the platting process. Platting shall include a compliance review with all applicable master plans, including the potential development of new master plans (see items 9.a and 11 below, as well as review of roadways and utility infrastructure.
5. Owner/Developer shall, upon finalization of the comp plan amendment and rezone, submit to City for review and approval a preliminary plat which identifies mega lots and proposed phases. This application shall include submittal of a study for buildout impacts and transportation needs as well as initial major infrastructure required upon implementation of each phase or mega lot. The study shall look specifically at required sewer main, water main, pressurized irrigation, and roadway infrastructure within the development which connects to adjacent city facilities off site, as well as intersections within the impact area. A utility and roadway master plan for the Project shall be included as part of this submittal. All infrastructure shall be sized as required for final build out and shall be based on a comprehensive review of existing infrastructure needs.
6. The parties recognize and Owner/Developer accepts that major infrastructure improvements will be required in order for the Conceptual Plan to be implemented consistently with the scope of this Agreement. The parties recognize that some infrastructure will be required immediately and other improvements may not be required until later phases. Owner/Developer accepts and shall construct the following as required infrastructure components:
  - a. Create a continuous four or five lane roadway (**“New Roadway”**) through the project that connects to the intersection of Idaho Center Boulevard and Franklin Road on the east and to Karcher Road on the west. Specific improvements include widening the current Ridgecrest Drive to four lanes from Idaho Center Boulevard to the eastern boundary of the Project; constructing a bridge from the western-most boundary of the Project over the irrigation canal and Union Pacific Railroad line to connect with Karcher Road.
  - b. Construct a north-south roadway from the New Roadway to connect with North 39<sup>th</sup> Street south of Interstate 84, specifically including a minimum two-lane

overpass over Interstate 84 complete with bicycle lanes and sidewalks per City requirements at the time of construction.

- c. Intersection improvements at Karcher Road and Franklin Boulevard including but not limited to signalization or construction of a roundabout.
  - d. Intersection improvements at North 39<sup>th</sup> Street and Flamingo including but not limited to signalization or construction of a roundabout.
7. The parties recognize and Owner/Developer accepts that roadway impacts of the Project extend well beyond the perimeter of the Project. Owner/Developer and City agree that at a minimum, twenty-one intersections and connecting roadways will be directly impacted by the Project. The following graphic identifies the intersections. Owner/Developer shall, at a minimum, address all of them in all Traffic Impact Studies (“TIS”) prepared in compliance with this Agreement.



- 8. Owner/Developer shall prepare a TIS consistent with City’s TIS policy whenever required by City as a component of each preliminary and final plat application noted above.
- 9. Owner/Developer shall at a minimum implement the following Water Utility improvements:
  - a. Install a network of mainlines through the development. Specific configuration shall be determined at the time of preliminary plats.
  - b. Install pressure reducing valves at the connections from the mainline network in the Project to the existing City water system at Karcher Road and 11<sup>th</sup> Avenue North.
  - c. Dedicate a 2 acre parcel for future water tank site; tank site shall be dedicated to the City by 2018. Site shall be located in the higher elevations of the development.

10. Owner/Developer shall implement one of the following Pressure Irrigation improvements:
  - a. Maintain the existing system as a private system and install a private distribution network to provide service to all lots; or
  - b. Upgrade the existing pressure irrigation facilities to meet City standards; installing public mainlines to provide service to all lots; and dedicating the system to the City.
11. Prior to submittal of a preliminary plat application, Owner/Developer shall engage in a sewer master planning exercise with the City of Nampa to help determine long range infrastructure needs associated with the implementation of this project. Costs associated with the sewer master plan update are estimated at \$8000 and shall be paid by the Owner/Developer for any planning associated with bringing the project on line.
12. Owner/Developer shall implement the following Gravity Irrigation improvement:
  - a. Provide for the continuation of all gravity irrigation supply and waste which enters and exits the site.
13. Construction of the overpass to E. Karcher Road shall be designed to perpetuate all existing driveway accesses.

**EXHIBIT “D”**

**DESIGN GUIDELINES**

# 03 DESIGN DOCUMENTS

## 03.2 DESIGN GUIDELINES

### 03.2.1 Introduction

These Design Guidelines have been created to provide an aesthetic road map in support of the Southwest Idaho Treatment Center (SWITC) Master Plan. Zoning Classifications set forth reference the Master Planning Summary discussing zoning and land use review per adjacent land uses. Ideally the SWITC property would eventually be rezoned with appropriate overlays in support of identified land uses. With this approach, the design guidelines defer to the Nampa City Zoning Ordinance guidelines for building setbacks, heights, and general zone uses. Throughout the document references to the Nampa City Zoning Ordinance anticipate a coordinated rezoning of the SWITC property for planned uses.

The guidelines include sections for site design guidelines, landscape design, golf course design, architectural design and signage. Each of the sections is specific to the building type described. A Design Review process shall be put in place so that all proposed work will be reviewed and approved according to development parameters, and maintained by the various owner associations within the development areas.



Site Design

### 03.2.2 Intent

The Design Guidelines are intended to be a timeless and flexible document with the understanding that development will occur over an extended time period. The goal is to ensure the development is harmonious, resulting in complimentary relationships between architectural forms and common site amenities.

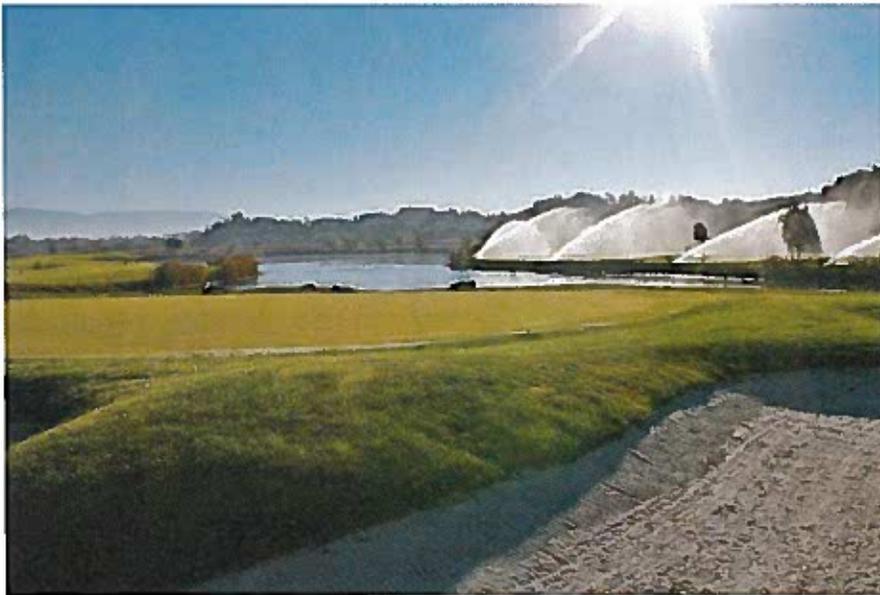
All improvements within the Southwest Idaho Treatment Center Master Plan will conform to the zoning and development established under a new Southwest Idaho Treatment Center Master Plan Ordinance. In addition to the Southwest Idaho Treatment Center zoning criteria and these design guidelines, projects will also be required to adhere to development covenants, conditions and restrictions (CC&R's). All future facilities must submit design documents to appropriate local jurisdictions and owner's association for review and approval. Further submissions as required by ordinance will follow City approval processes.



Architectural Design



*Landscape Design*



*Golf Course Design*

# 03 DESIGN DOCUMENTS

## 03.3 SITE DESIGN GUIDELINES

### 03.3.1 Introduction

The site design guidelines establish the framework with which to approach the design and planning of the various development areas within the SWITC Master Plan area. Included in this text are both planning components as well as implementation components. Placement of buildings is as important as the development of amenities throughout the development areas to ensure a consistent level of quality.

The objectives of the site development guidelines are:

- *To support and amplify the goals of the Southwest Idaho Treatment Center Master Plan*
- *To recognize the unique nature and location of the property within the local context as well as within the context of the entire Treasure Valley.*
- *Responding to the area's uniqueness through the enhancement and creation of an interesting and aesthetically pleasing environment.*
- *To encourage development that is visually understandable and meaningful to the users.*
- *To encourage planning and buildings of a high quality and appropriate character while maintaining a variety of expression and creativity within various areas of the development.*
- *To promote pedestrian accessibility throughout the development and connectivity to adjacent uses.*
- *To create a pedestrian scale in the design of streets, spaces between buildings, and the buildings themselves.*
- *To enhance the appearance of buildings and structures through site design.*
- *To create visual unity and continuity among parcels, neighborhoods, and adjacent properties.*

### 03.3.2 Land Use

The SWITC Master Plan includes a design approach which responds to the site's terrain, views, amenities, and access. Those land use zones identified below shall be translated into zone overlays through the City of Nampa. The overlay zones within the areas include commercial, mixed-use, professional office, and residential.

The land uses with the SWITC Master Plan include:

- *Residential – to range from low density single-family residential to high density multi-family residential*
- *Commercial – include dense mixed use urban core with a variety of uses including commercial retail, office and residential. The intent of higher density is to create greater access to shopping, recreating and working through a pedestrian friendly community. Other commercial uses within the plan include a lower density business community, and commercial retail (banks, restaurants, shops, etc.).*
- *Job Corp Campus - Existing*
- *Institutional (Educational)– campus with open space*
- *Civic*
- *Public – Golf course and clubhouse, recreational park*
- *Transit Hub – connecting the surrounding communities to the SWITC property and providing alternate modes of transportation.*



Conceptual Land Use Plan

*Legend*

Commercial/Office		Single Family Residential	
Hotel		Multi-Family Residential	
Commercial Campus		Retirement	
Mixed-Use		Storage	
Civic		Golf Fairway and Greens	
Retail/Restaurant			

# 03 DESIGN DOCUMENTS

## 03.3 SITE DESIGN GUIDELINES

### 03.3.3 Site Development

The site development of areas within SWITC will provide a visually distinctive and memorable experience to its users and residents. The overall concept for the site planning is to capitalize on the site's existing amenities, its accessible location, its visibility from travel corridors, its mountain views, and reconfiguration of the public courses to continue the tradition of quality golf experience while allowing inclusion of a planned development with diverse uses from residential to commercial, creating a vibrant addition to Nampa and its surrounding areas.

Related site improvements include but are not limited to parking, a transit hub, walkways, street networks and associated amenities including lighting, as well as pedestrian amenities. Open space amenities includes public gathering areas, plaza and parks, and golf course connectivity. The resulting site configurations create a series of large outdoor spaces connected by structure

placements, roads, and walkways throughout the Southwest Idaho Treatment Center Master Plan areas.

The land use designations are a deliberate mix of commercial office, commercial retail (including banks, restaurants, and shops), mixed-use, multi-family housing hospitality, civic, and single family residential. The mix is designed to create a vibrancy and livability with multiple opportunities for each. The Master Plan also responds to establishing appropriate buffers and adjacencies for these residential districts providing support for the residents with working, shopping, and recreating opportunities

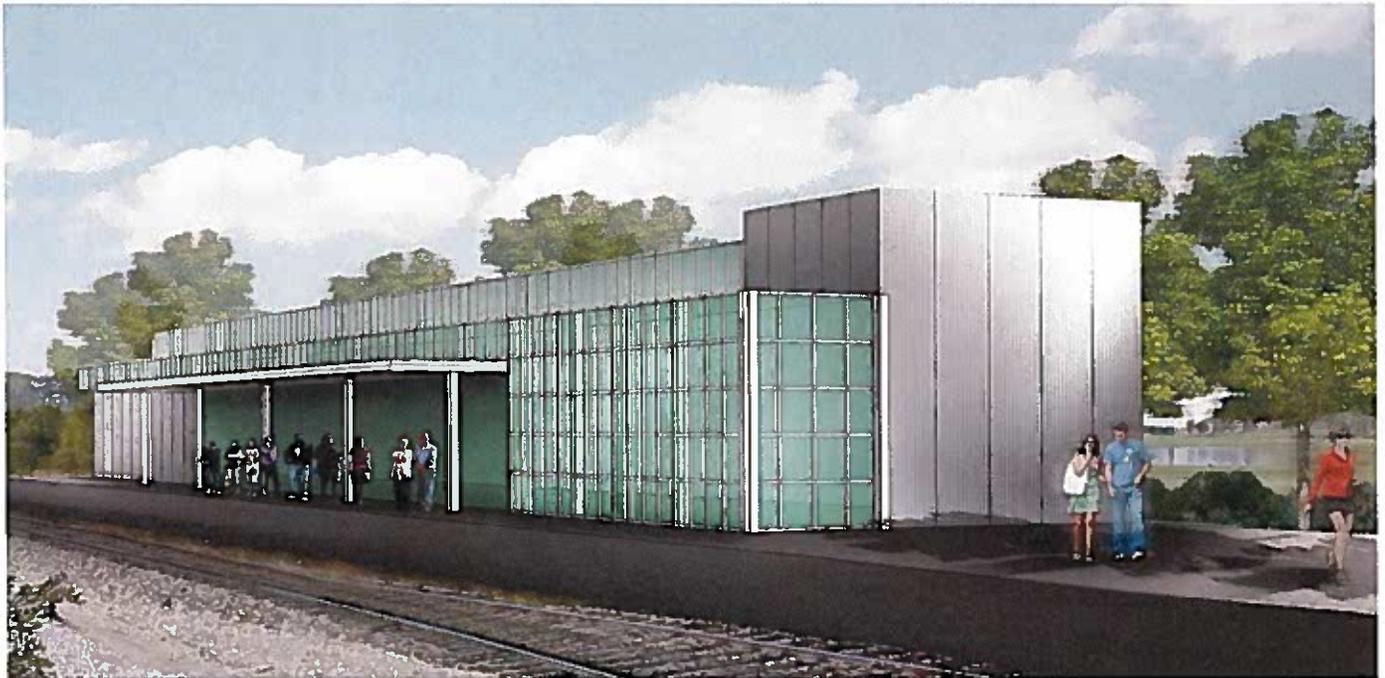
Distinct residential districts will provide opportunities to accommodate multiple living styles depending on future residences' necessities. The following images demonstrate some of the required characteristics of site development at SWITC including spatial relationships between building, public open space, architectural styles and landscape. (See Images 1-8)



1. Commercial Campus Aerial



2. Commercial Campus Perspective



3. Transit Hub

# 03 DESIGN DOCUMENTS

## 3.3 SITE DESIGN GUIDELINES



4. *Mixed-Use Perspective*



5. *Mixed-Use Aerial*



6. *Single Family Residential Aerial*



7. Multi-family Residential Aerial



8. Multi-Family Residential Perspective

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03.4.1 Introduction

The term landscape as used in this section refers to those elements that give form and character to the physical and natural environment of the Southwest Idaho Treatment Center (SWITC) project site. Those elements include but are not limited to the placement of trees, shrubs and other vegetation, sidewalks, paths and trails, drainage swales, lighting, screening, entry features, open space, plazas and patios. The goal of this section of the design guidelines is to provide design standards that create aesthetically pleasing outdoor spaces, with functional and safe circulation systems for pedestrians and vehicles.

The landscape design for the SWITC project area should respond to the character of the site and create places with different spatial characteristics. Soft and hard landscape materials shall be selected that relate to the urban nature of commercial and mixed use development, or park-like feel of recreational facilities, or the natural character of the areas that buffer the golf course facilities. The landscape design guidelines for public right of way and for vehicular and pedestrian corridors within the SWITC project area reflect several important objectives:

- *To establish consistency in the design of streets within the SWITC project area.*
- *To create a strong visual identity through street design that provides unity to the project and a framework for individual expression in the design of specific project areas and parcels.*
- *To create a pleasant pedestrian environment with universal accessibility and safety throughout the project area.*
- *To improve traffic flow and safety in the entire SWITC area.*



*Color and Texture of Plant Materials*

### 03.4.2 General Landscape Requirements

The following are general requirements that apply to landscape design for all development within the SWITC project area:

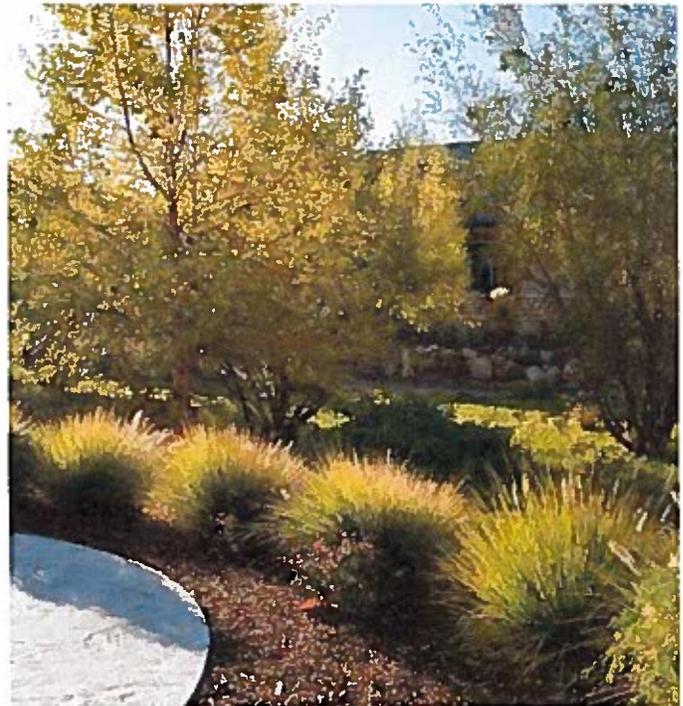
- Provide year-round interest in the selection of plant material, through varied use of color, texture and form and deciduous and evergreen plants.
- A landscape plan is required for all parcels and shall be prepared by a licensed landscape architect/designer. Plans should identify all plant material including names, sizes and quantities, hardscape improvements, proposed and existing structures, grading and drainage, and irrigation plans.
- Planning for water wise landscaping is encouraged through plant selection, irrigation design, water harvesting, and utilization of alternative water sources beyond municipal systems.
- All plant material shall meet minimum standards of ANSI Z60.1, American Standard for Nursery Stock, and shall be warranted for 1 year from the date of substantial completion.
- All plant material shall be planted in accordance with the

latest version of the standards by the American Nursery and Landscape Association.

- Bark mulch and weed barrier fabric shall be applied to a depth of 3" in all planter beds for weed control and moisture retention. Rock mulch will be allowed on a case by case basis where the mulch is part of the aesthetic design.
- Landscape areas may not have slopes greater than 3:1, and lawn areas shall be on a slope of 4:1 maximum.
- Primary pedestrian pathways through open space shall be 8' width and paved. Secondary pathways from the primary pathways to building entrances, parking lots etc may be 5' width.
- All trees shall be planted outside of utility easements provided for municipal water mains, sewer mains or storm water facilities. Trees shall also be located outside of all Idaho Power easements.
- Landscaping within or adjacent to road intersections, driveway intersections and roundabouts shall comply with landscape guidelines while maintaining clear vision triangles for both pedestrians and vehicle traffic. Local codes should also be referenced in regard to vision triangles to ensure that all safety requirements are achieved.



Year-round Interest



Bark Mulch in Landscape

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03.4.3 Landscape Standards for Streets

A coherent and organized streetscape is important in providing beauty and order within this development. The purpose of this section is to provide a clear basis for selecting the appropriate type of trees and landscaping for each type of street within the SWITC Master Plan area. The visual importance of street trees is critical to the identity of the area, as they provide visual coherence between streets, buildings and open space. Tree form, foliage, color and fragrance create identities along with uniqueness. The following guidelines build upon the existing local codes by offering some variation to enable the creation of distinctive spaces through careful selection and placement of trees, shrubs and groundcovers.

#### 01 COLLECTOR AND ARTERIAL STREETS

- Construction standards and dimensions of right-of-way, drive lanes, bike lanes, center islands, sidewalks and landscape strips shall be in accordance with standards shown for Collector and Arterial Streets and shall comply with the local highway district.
- Class II deciduous shade trees are to be planted within the landscape strip between the back of curb and sidewalk along all collector or arterial streets. Tree spacing shall be 35 feet on center. (See figure 01.)
- Species should remain the same along each road until reaching intersections or landscape features where transitions to another species may be appropriate.
- All street trees shall be minimum 2" caliper at the time of installation.
- Buffers outside of the streetscape-planting strips shall comply with local jurisdictional requirements for landscape buffers and shall consist of lawn, shrubs, groundcover and trees. Properties fronting any street shall provide the minimum required landscape buffer between the back edge of the sidewalk and edge of any site development (parking lot, building, etc.). (See figure 01.)
- Landscape buffers shall consist of a combination of planting, berms, walls, or fences that provide a minimum 36-inch and maximum 42-inch visual screen between adjacent parking areas and public streets. (See Parking Screening section of in Landscape Guidelines.)
- Pedestrian crosswalks, which cross streets or major driveways, shall consist of a material other than asphalt. Approved materials include concrete and concrete pavers. Where concrete crosswalks abut asphalt streets or

driveways, a proper transition, such as a concrete apron, is required between the concrete and asphalt to avoid excessive wear and damage. (See figure 01.1)

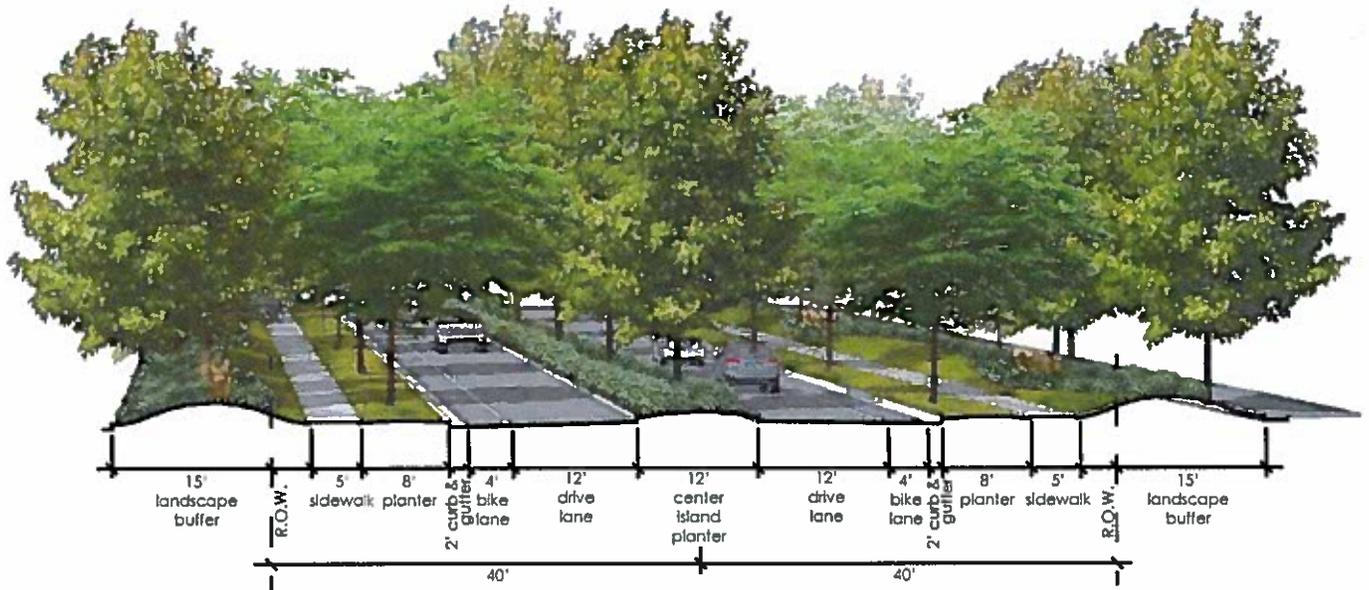
- Maintenance within the public right of way including street trees, groundcover areas, irrigation and sidewalks will be paid for by funds supplied by HOA dues.
- Trees adjacent to public pedestrian walkways in the landscape buffer can be class I, II, or III provided that they do not interfere with pedestrian circulation. Class I and II trees shall be a minimum of 4' from the edge of sidewalk or curbs, and class III trees shall be a minimum of 8' from the edge of sidewalk or curbs.
- Center planter islands shall be placed to avoid creating sight line obstructions. Refer to local codes for requirements of sight lines at controlled and uncontrolled intersections.

#### 02 MIXED-USE AREA STREETS

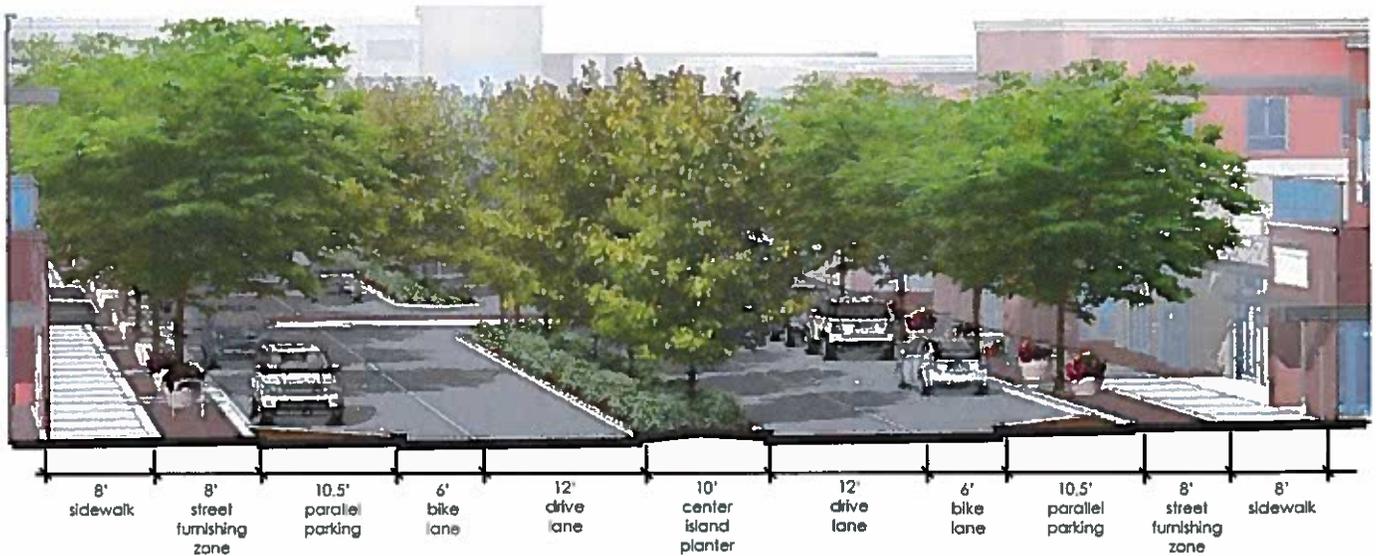
- Construction dimensions of drive lanes, bike lanes, center islands, sidewalks and parking lanes shall be in accordance with the standards Mixed-Use Area Street Section identified in the guidelines. (See Figure 02)
- Class II deciduous shade trees are to be planted along all streets through the mixed-use zone at approximately 35-foot intervals in the street-furnishing zone between the curb and sidewalk. Trees shall be planted within tree grates.
- Species should remain the same along each road until reaching intersections or landscape features where



01.1 Pavers Crosswalk with Concrete Apron



01 Collector and Arterial Streets Section



02 Mixed-Use Area Streets Section

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

transitions to another species may be appropriate.

- All trees shall be minimum 2" caliper at the time of installation.
- All benches, trash cans, bike racks and planter pots shall be placed in the 8' wide street furnishings zone. Configuration of street furnishings shall be similar to image below (See Street Tree Planting and Site Furnishing illustration below.) See site furniture portion in this document for further details.
- The 8' wide street furnishings zone shall be comprised of a material other than standard concrete. Alternative solutions include brick or concrete pavers or colored and stamped concrete. Once a style is established, it shall remain consistent. See site furniture and paving sections in this document for further details.
- Pedestrian crosswalks within this zone shall be paved with

colored/stamped concrete or concrete pavers. Where concrete crosswalks abut asphalt streets or driveways, a proper transition such as a concrete apron is required to avoid excessive wear or damage.

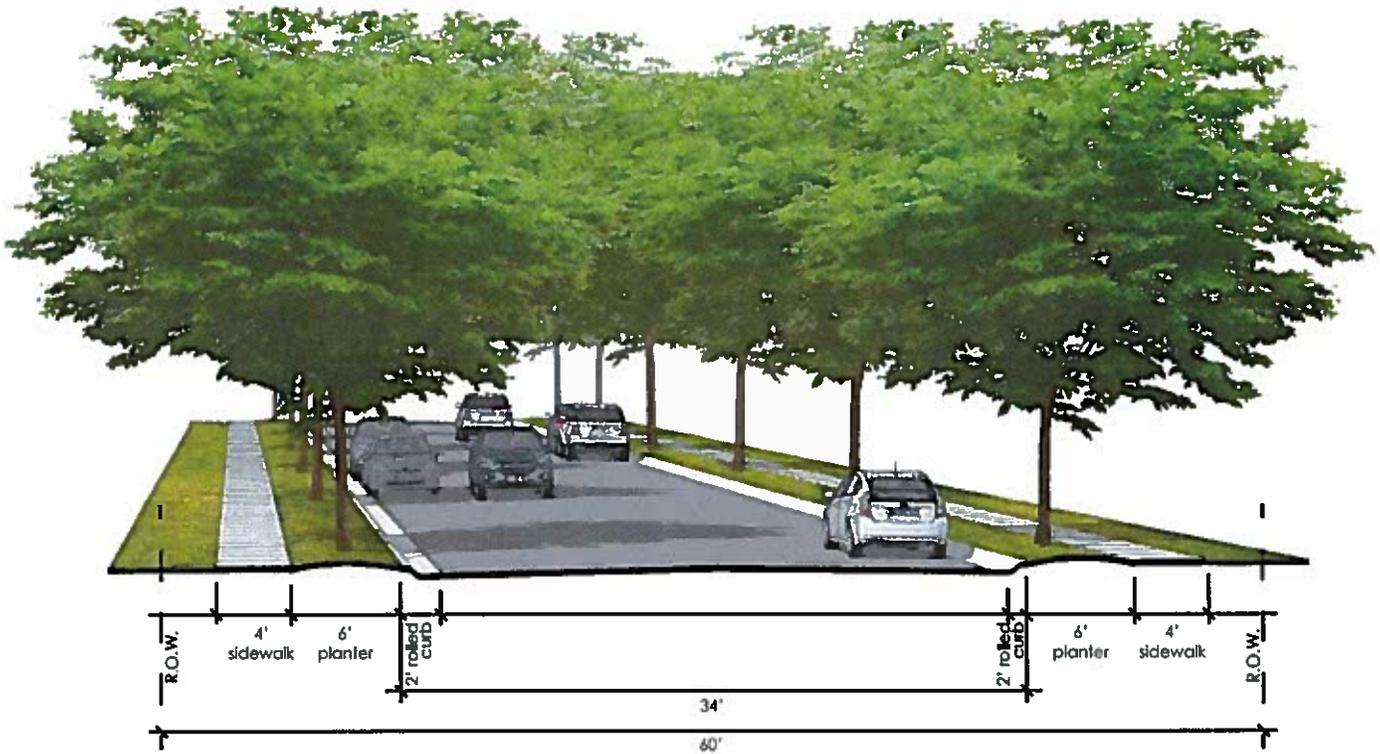
- Maintenance within the public right of way including street trees, groundcover areas, irrigation and sidewalks will be paid for by funds supplied by HOA dues.

### 03 RESIDENTIAL STREETS

- Construction standards and dimensions of right-of-way, drive lanes, parking lanes, sidewalks and landscape strips shall be in accordance with local jurisdictional standards for Residential Streets.
- Class II deciduous shade trees are to be planted along all streets in Residential zones at approximately 35 foot intervals in the planter between the curb and sidewalk.



Street Tree Planting and Site Furnishing



03 Residential Streets Section

- *Species should remain consistent along each road until reaching intersections or landscape features where transitions to another species may be appropriate.*
- *All trees shall be minimum 2" caliper at the time of installation.*
- *The homeowners association is responsible for maintaining the improvements within the public right of way from the property line to the edge of the curb. This includes street trees, turf, irrigation and sidewalks.*
- *The entrances to residential areas are to be defined with appropriate decorative entries which may include planting, signage, water features, masonry structures or other elements of interest.*

# 03 DESIGN DOCUMENTS

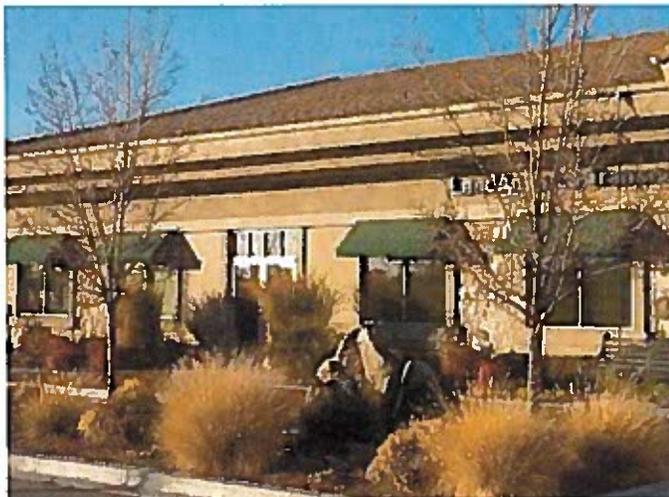
## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03.4.4 Landscape and Planting Design

The SWITC site encompasses a large geographical space and several different proposed uses. Within each zone of development, there is a requirement for the landscape design to create spaces that are aesthetically pleasing while providing safe and unique gathering places that transition well from one space to the next. Success of this approach will depend greatly on the appropriate placement of landscape features, with an intent focus on the relationship of these elements and proposed or existing structures.

Landscape themes typically have relationships with certain plant types. Whether it is urban, residential, ornamental, natural, or transitional type themes; the types of trees, shrubs, groundcovers and grasses should reflect the desired context. Several landscape types are identified within these general guidelines along with planting schemes that promote the creation or enhancement of place. The use of semi-mature plant materials is encouraged to establish the character of the development.

Similarly, hardscapes such as plazas, patios and pathways help define specific themes. Materials, shapes, furnishings and size are just a few characteristics that help achieve thematic success. Public spaces should contain seating, sunny and shady areas, and places for social interaction or quiet respite. Circulation systems should be accessible, safe and provide access to all parts of the site through a hierarchy of paths that accommodate expected traffic. Building plazas should provide a clear sense of arrival and entry, and pathways between buildings should satisfy desired travel paths.

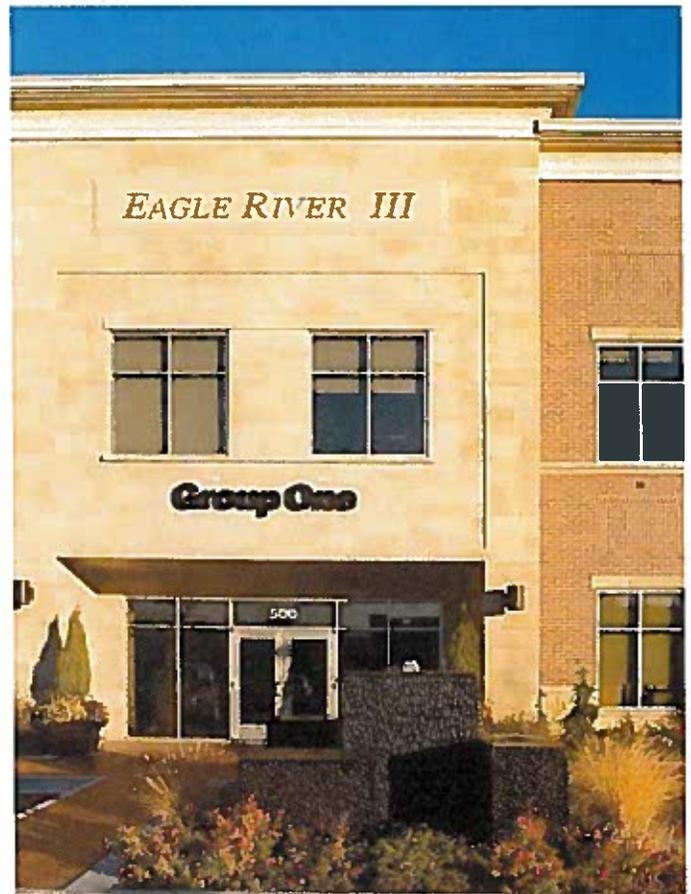


01 Foundation Plantings

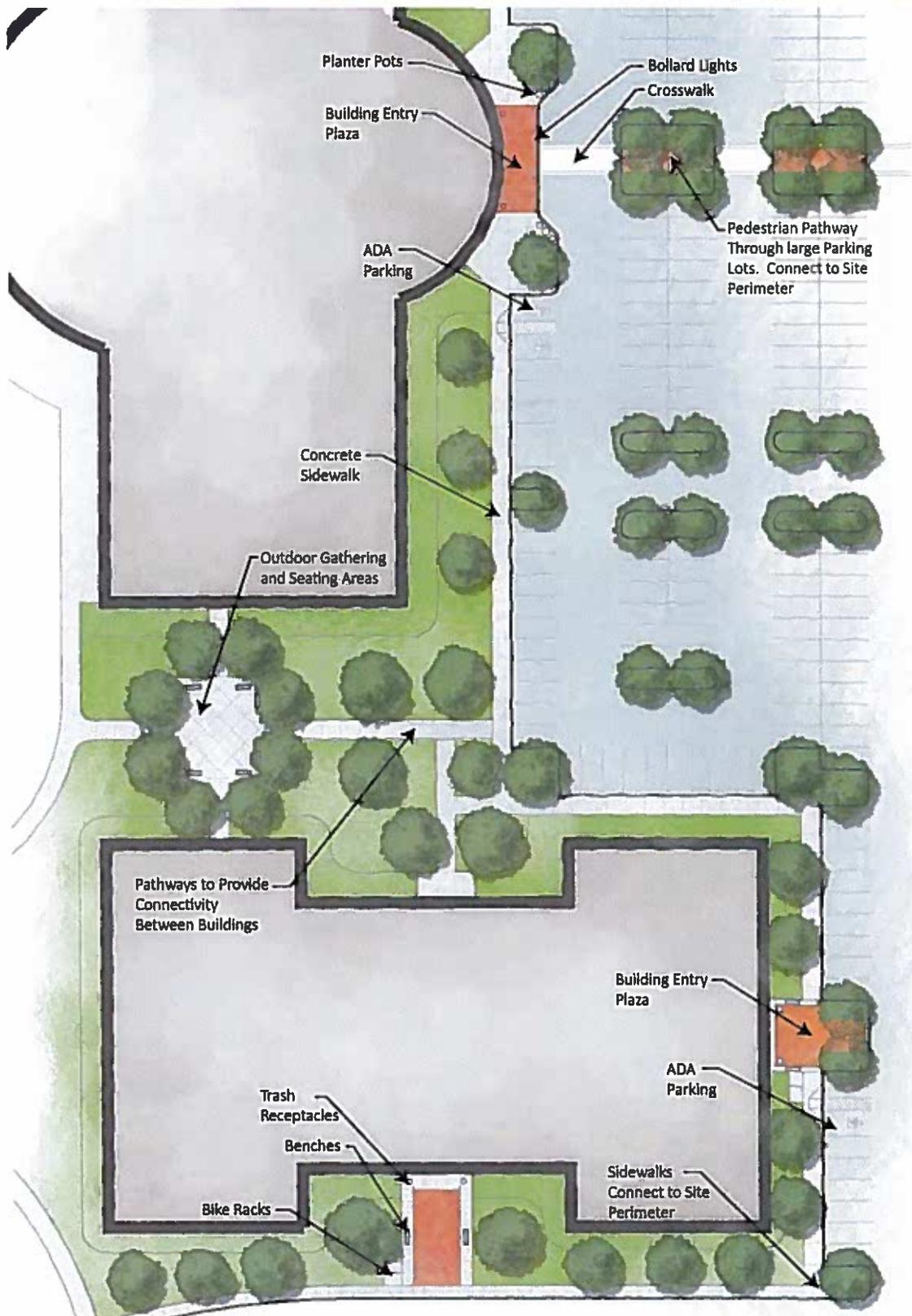
### 03.4.5 Landscape Guidelines for Use Zones

#### 01 CAMPUS, OFFICE AND COMMERCIAL

Landscape planting around the campus, commercial and office buildings helps to blend different architectural styles, provide color and texture in the environment, soften the hard edges between structures and the ground plane, and accent the visual appeal of architectural style. Buildings within these zones shall have foundation plantings around the majority of building perimeters at a width not less than 6'-0", but proportionally adapted to vertical building heights. Plant material utilized for foundational planting should be easy to maintain, should thrive both visually and physically in large massing scenarios, and shall adapt to sun angles created by the vertical building walls. Large massing's of plant species are recommended for foundation planting scenarios; however, building entries shall be accentuated through the use of perennials, specimen plants, art features, water features or plaza spaces. Building corners and architectural features shall



01 Entry Planting



Conceptual Example of Design and Layout of the Site Furniture, Walkways and Paving in Commercial Development

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

also be accented through the use of specimen trees or shrubs.

To ensure that design intent is effective, all shrubs, perennials and groundcover in mixed use and urban areas should be maintained in their natural state, to the sizes and shapes identified in the species selection list. (See Appendix E) Maintenance, specifically pruning practice, plays an important role in the long-term success of a landscape. Except for in rare instances where bonsai or shaping practices are being implemented, plants shall be pruned and maintained in their natural forms.

Hardscape shall be implemented at main and secondary entryways to buildings. Main entry points shall promote a sense of arrival and provide for small gatherings through the use of varying paving materials and site furnishings. Site furnishings shall include items such as benches, trash containers, and bike racks. Decorative and functional bollards, landscape lighting and planter pots should be considered to enhance paved areas in terms of function and aesthetic appearance, but should not clutter or impede entrances or pathways. See guidelines for Site Furniture, Walkways and Paving in this document for further details.

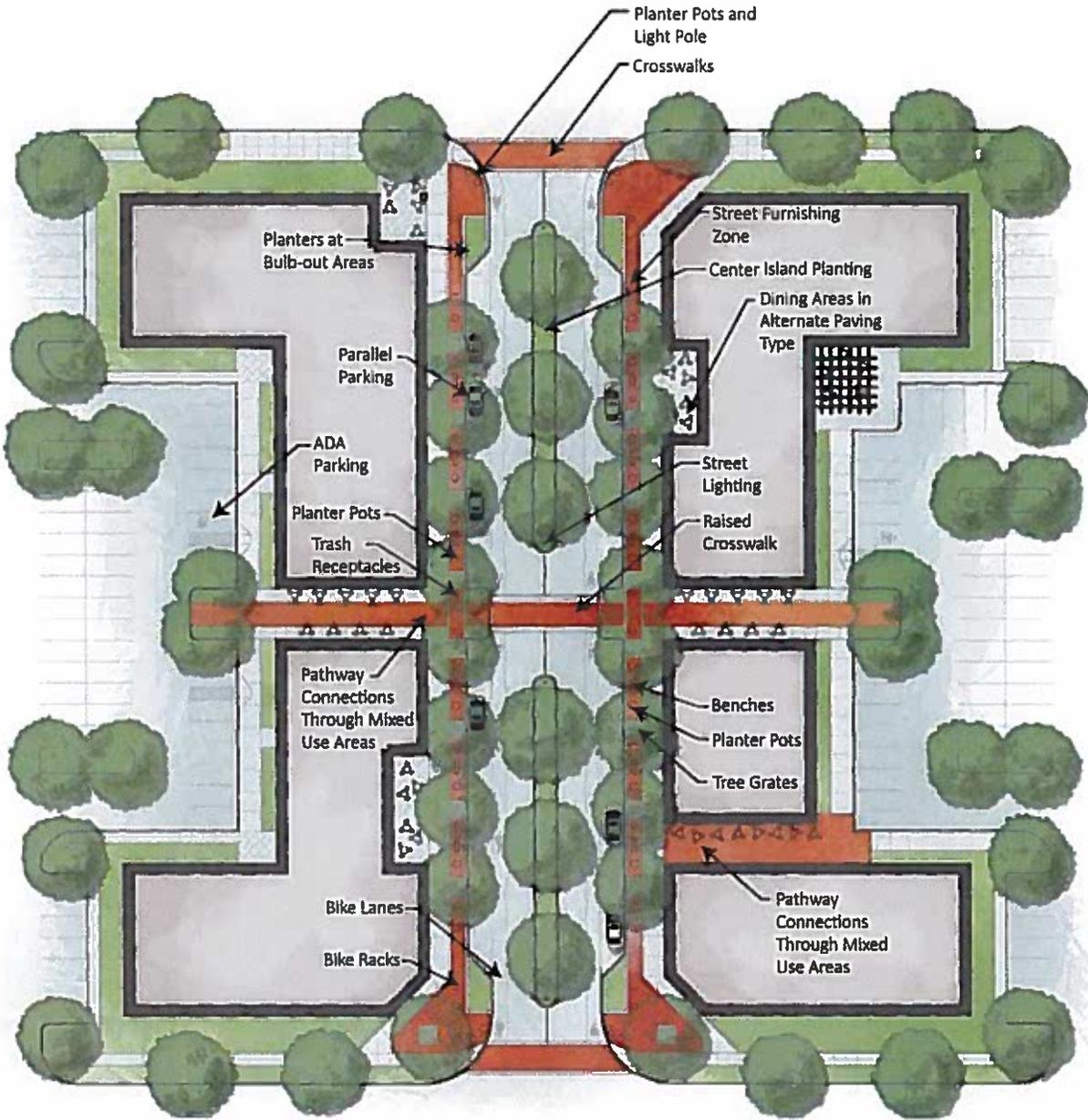
### 02 MIXED-USE AND URBAN

The mixed use areas of the SWITC Master Plan are intended to provide an urban lifestyle center centrally located within the development. Landscape design within this area will focus primarily on hardscapes and site furnishings. Paving design shall delineate between primary walkways which allow unimpeded pedestrian travel ways and access to entries, and the site furnishings zone adjacent to the street. Other elements to be provided in the mixed use and urban zone include outdoor dining areas, bike lanes and parallel on-street parking.

The general approach to all planting should be to select species that contribute to the visual experience of the public as they use this area, whether street trees, planters, hanging baskets, or containers. Planting schemes should provide diversity through color, form, texture, and fragrance. Softening of the built landscape, such as buildings and pavement, is encouraged. Plants that provide year-round interest with changing foliage, color or spring flowering should be provided, as well as sufficient evergreen plant material. To ensure the design intent is effective, all shrubs, perennials and groundcover in mixed use and urban areas should be maintained to the height and width as specified in the *Plant Species Selection List*. (See Appendix E)



02 Urban Site Furnishings



Mixed-Use Area Showing Conceptual Planning, Paving, Street Furnishings Zone and Pathway Connections

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03 COMMUNITY PARKS, OPEN SPACE, AND TRANSIT CENTER

Higher pedestrian use areas of a park may require selection of plant material that is more robust and functional, particularly if placed adjacent to paved or high use areas. Areas of lesser use such as small gardens, may include a higher proportion of specimen plant material. In all areas, planting schemes should provide diversity of color, texture and form, and shall accent adjacent hardscapes or structures

Concepts of Crime Prevention Through Environmental Design (CPTED) should be employed to ensure that design in these areas deters potential criminal activity. This shall be accomplished by promoting natural surveillance, controlling access points, creating a sense of ownership, and through routine and consistent maintenance. CPTED landscaping guidelines shall be used, including planting shrubs with a maximum height of three feet and trees with a proper ground clearance of eight feet above walkways, vehicular travel and parking lanes. To ensure the design intent is effective, all shrubs, perennials and groundcover in these areas shall be maintained to maximize beauty and minimize hiding areas.

### 04 GOLF PERIMETERS

The areas around the fairways, tees and greens, and the steep slopes of the escarpment to the north of the site, are maintained minimally with little or no irrigation, and contain both native and non-native grasses and shrubs. Design proposals for these areas are limited, or are intended to remain undisturbed. Planting in developments bordering these naturalistic areas should blend from ornamental to native to maintain the existing condition and character of these areas. Shape of planter beds should be organic in nature and tree spacing should be irregular. Species selection should be from *Plant Species Selection List*. (See Appendix E, pg. 132) All landscaping adjacent to tees, fairways and greens shall be approved by the Golf Course Architect.



03 Planting at High Pedestrian Use Areas



04 Planting at golf Perimeter

#### **05 RESIDENTIAL**

The SWITC Master Plan project includes residential development of varying densities and diverse architectural forms, including single family residences, apartments, townhomes, and a retirement community. Landscape design in these areas should respond to the context of each residential area. High density areas may be more urban in feel and so may borrow more from the guidelines in the section for urban landscape design, whereas low density areas may relate more to adjacent natural landscapes and employ the use of perennials or native planting on a greater scale. Groups of plants may be smaller creating more of a garden feel although this does not preclude larger concentrations of ornamental grasses or perennials for example. Gathering or sitting areas should include a good diversity of plants that provide year round color, shade, separation from roadways, and contrast of shape and texture. Residential developments should be designed with unique characteristics to create places that foster a feeling of individual identity for the local residents. Species selection should be from *Plant Species Selection List*. (See Appendix E, pg. 132)

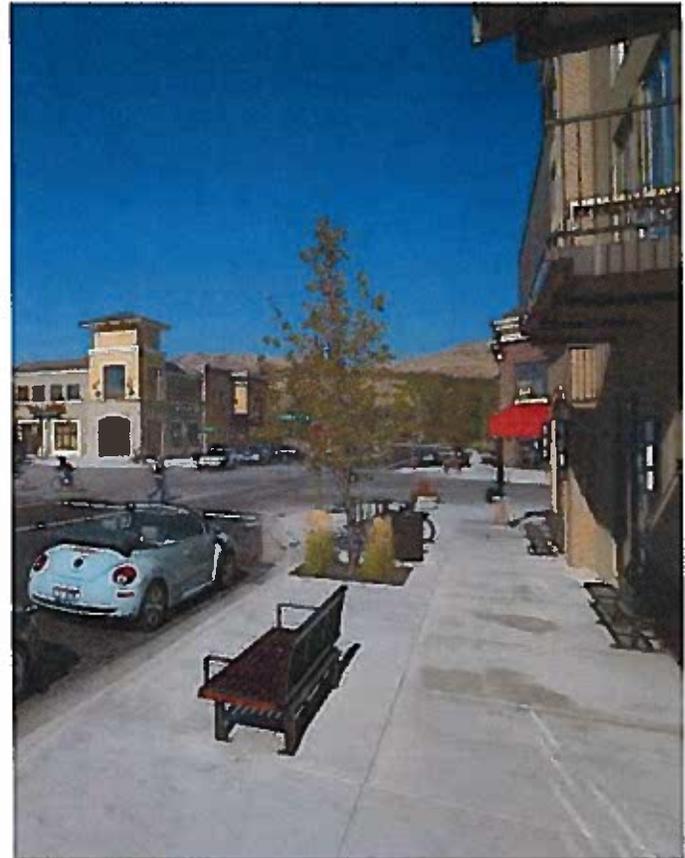
# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03.4.6 Site Furniture

Site furniture shall be provided to increase the aesthetic quality, enjoyment and safety of plazas, walkways and public open spaces. These include, but are not limited to: benches, planter boxes and pots, trash receptacles, bollards, bollard lighting, bike racks, picnic tables, shelters, overhead shade structures and tree grates. While product manufacturers, materials or colors are not specified in these guidelines, the design and selection of these components of the landscape should be complimentary to other site furniture in the development, to the buildings to which they relate in terms of their material, color, form etc., and to the context of the surroundings, whether it be an urban hardscape or park or trail location. Particular attention shall be paid to site furniture in the direct vicinity of core areas such as the mixed use development, where materials, colors etc. shall be consistent and complimentary with the design standards already in place. Numbers of bike racks provided shall meet the requirements of local ordinances for the appropriate zoning category.

Site furniture should be placed appropriately, taking care to minimize visual clutter. Placement within plazas or other gathering spaces, along walkways and outside of buildings should be based on the use of the space and the anticipated flow of pedestrian or vehicular traffic, and to ensure safety of the space being used. Furnishings should be placed to anticipate requirement for both sun and shade, and other climatic conditions that affect use of outdoor spaces. The mixed use zone streets are required to have a street furnishings zone where all benches, trash receptacles, bike racks etc should be located. (See adjacent image)



Site furnishings

### 03.4.7 Walkways and Paving

One of the overall aims of the site design guidelines is to create a walkway and path system that will connect all parcels and buildings within the SWITC Master Plan project area. Parcel development shall include:

- Provision for paved entry areas or plazas at building entrances. Sized to accommodate expected pedestrian traffic and to relate to scale of buildings. Include street furniture as required.
  - Provision of paved gathering spaces adjacent to and between buildings for gathering or socializing opportunities for employees etc.
  - Provision of one or more walkways that directly links the pedestrian entrances of businesses within the commercial development to public pathways adjacent to streets.
  - Interconnection of development parcels and buildings
- by providing pathways along direct desire lines to other buildings, plazas, open space or walkways.
- Delineation of walkways within the commercial development from parking area paving by using a contrasting paving material. The material shall be complimentary in terms of color, texture and material to the surrounding buildings and context.
  - Walkway surface patterns and scoring depth that are compatible with the comfort and safety needs of pedestrians, especially the elderly and the handicapped.
  - Particular attention shall be paid to paving design and layout in the direct vicinity of core areas, where materials, patterns etc. shall be consistent and complimentary with the design standards already in place.
  - Interconnection of adjacent buildings by providing clearly



*Contrasting Paving Material*

*marked pathways both to the primary pedestrian pathway and from building to building.*

- *Walking trails through the SWITC development should connect different parcels and land uses to promote connectivity throughout the project area. All trails should be paved and be a minimum of 5' width, and provide seating at appropriate resting areas.*



*Gathering Space and Entry Area*



*Walkways Connecting SWITC*

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03.4.8 Parking Lot Design

#### 01 LAYOUT

Layout of parking lots should meet the requirements of the local ordinance. Minimum standards for SWITC include:

- All standard parking spaces to be 9'x20'.
- All accessible parking spaces and access routes must meet requirements of A.D.A. Accessibility Guidelines for Buildings and Facilities (ADAAG).
- Accessible parking spaces serving a particular building shall be located on the shortest accessible route of travel from adjacent parking to an accessible entrance.
- Rows of parking spaces should not exceed (12) stalls before the placement of a landscape planter. Parking lot planter islands shall be a minimum width of 9' including curbs.
- A minimum 5' width concrete sidewalk shall be provided along the edge of all parking lots where adjacent to a building façade.

#### 02 PLANTING

Planting in parking lots is required to soften the visual impact of large expanses of paving, to provide shade for cars and buildings, and for other positive environmental effects such as mitigation of storm water run-off. Minimum landscape planting requirements for parking lots shall be as described in local ordinances. Specific requirements include:

- Provide class II sized trees within all islands at the density described in the Nampa City Code. No Class I trees will be allowed within parking lot planters.
- Class III trees may be located in parking lot planters where the planter size allows the trunk to be at least 8' from the edge of curb.
- Use a species of tree that will permit initial limbing of seven (7) feet height. Prune trees regularly to achieve an ultimate limb height of twelve (12) feet.
- Locate the trees to frame building entryways and signage.
- Protect trees from overhanging bumpers with concrete curbs and allow for a minimum of four (4) feet between the curb and the center of the tree trunk.
- Use one tree species in parking lot areas that are defined by a group of buildings or separated by drive aisles. For variety, vary tree species between parcels.

- Islands shall be planted with shrubs, perennials or ground-cover to match the species type for Campus/Office/Commercial/Residential etc.

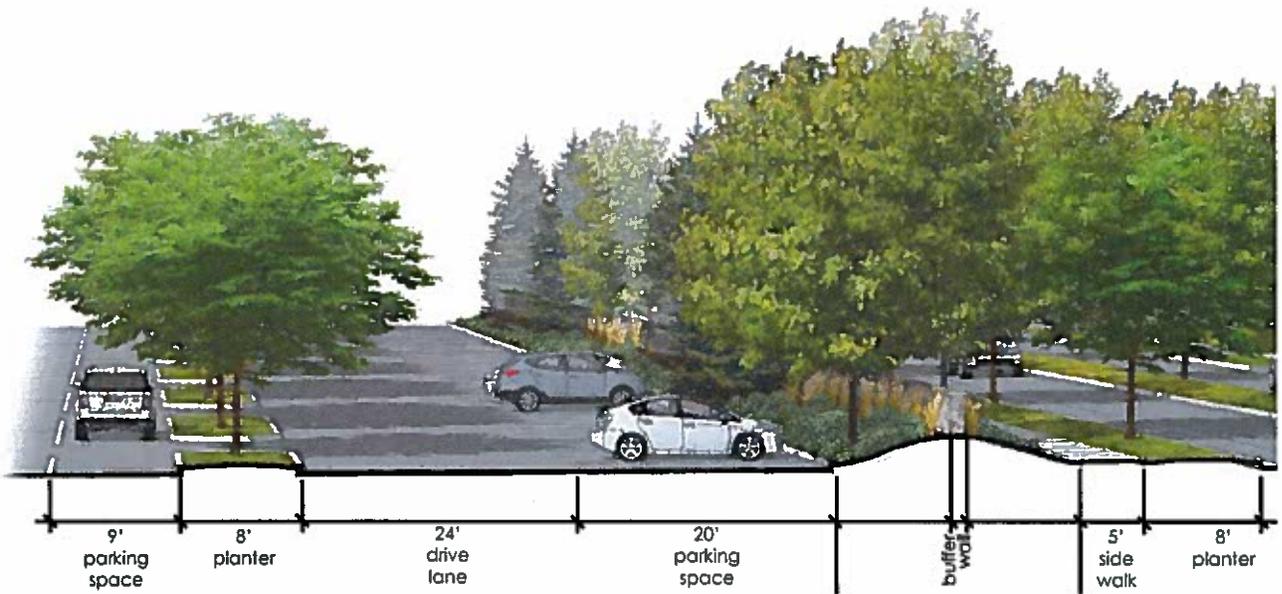
#### 03 SCREENING

To reduce the visual impact of rows of parked cars, parking lots are to be screened from view where they border public streets. This includes roads within the multi-family and retirement community areas that access off-street parking lots. Maximum screen height shall be 42 inches. The following requirements pertain to screen design:

- Screens may consist of all plant material or a combination of low walls, earth berms, and supplementary plant material.
- The plant material in all-plant screens should be of such a type and number to reach a height of 42 inches within three years and to be approximately 75% opaque year round.
- Designs for wall screens shall include some low foundation plant material to visually soften the wall.
- Walls may be constructed of wood, masonry, or concrete, but must be complimentary to any adjacent buildings and in context with the surrounding landscape.

Drive thru lanes can also be visually intrusive and require the following considerations:

- Wherever feasible, orient the drive through lane to be perpendicular to public streets to reduce headlight glare into oncoming traffic.
- Visually screen drive through lanes from view along public streets. Screening may be accomplished using plant material or a combination of low walls or earth berms and supplementary plant material as described for parking lot screening.



*Parking Lot Screening Section*



*Drive Thru Screening*



*Drive Thru Screening*

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

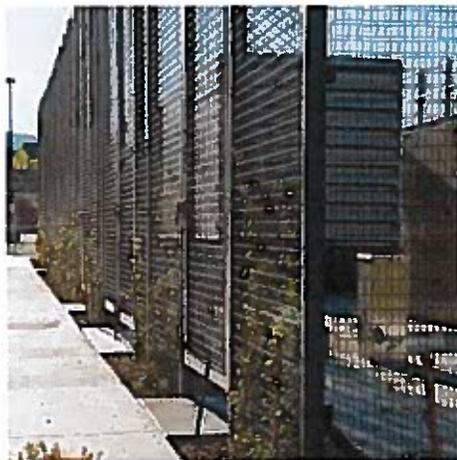


Storm Water Bioswale

### 03.4.9 Storm Water Treatment

Storm water treatment should meet the minimum design standards of the City of Nampa code, complying with the City Engineering Development Process and Policy Manual, and Standard Construction Specifications documents. All storm water generated on parcels of the SWITC development shall be retained on site, directed into appropriate permanent storm drainage facilities such as seepage beds and swales.

Swales should be designed to blend into landscape areas avoiding geometric shapes and steeply graded sides (maximum 4:1). They should be planted with grasses and shrubs to reflect both the growth conditions likely to be present and the character of naturally wet areas, while not impeding the capacity of the swale to dissipate storm water. Boulders and cobble rock may also be used as elements of the design of swales. Swales shall be designed to be free-draining with no standing water within 24 hours of the end of a storm water event. The use of permeable paving is also encouraged for plazas, walkways and parking lots where appropriate.



Service Area Screening

### 03.4.10 Service Areas

All service areas are required to be screened from public view. This is to be achieved as follows:

- Screen dumpsters, trash cans and recycling bin locations from public view with a combination of screen walls and plant material. Walls should be of a material that is complimentary to adjacent buildings.
- The height of plant materials and walls should fully screen the dumpsters, trash cans and recycling bins at the time of installation.
- Loading dock areas should also be screened from public view with landscaping and/or walls.
- See architectural guidelines for additional information.

### 03.4.11 Water Use and Irrigation

All landscape areas shall have an automatic underground irrigation system. An irrigation plan is required for all parcels to be developed prior to construction with information that shows at minimum:

- Irrigation performance specifications including design requirements, materials and construction methods.
- Head layout, sleeve, pipe, and valve sizing and locations.
- Backflow preventer and controller types and location.
- Available gallons per minute, water pressure and point of connection.

The performance specifications shall address the following requirements:



Screening Dumpsters



#### *Automatic Underground Irrigation*

- *Specify an appropriate backflow prevention device.*
- *The irrigation shall be designed to provide 100% coverage with head to head spacing.*
- *Sprinkler heads shall have matched precipitation rates within each control valve circuit.*
- *Lawn areas and shrub areas shall be on separate irrigation zones.*
- *Use water management principles to irrigate efficiently with properly designed systems and by applying the right amount of water at the right time.*
- *Maintain the landscape appropriately by mowing, pruning and fertilizing properly.*
- *Utilize smart controllers and weather stations to maintain efficient watering times.*

Water-Wise landscapes promote conservation of water, use of indigenous plant species and reduced water costs associated with landscape irrigation. The following design considerations should be employed as practicable:

- *Group plants of similar water needs together to reduce overall requirement for water.*
- *Use native plants or drought tolerant species, where appropriate.*
- *Use soil amendments such as compost or manure for moisture retention.*
- *Ensure bark mulch is maintained to a sufficient depth.*

# 03 DESIGN DOCUMENTS

## 03.4 LANDSCAPE DESIGN GUIDELINES

### 03.4.12 Fences and Walls

Fences or walls may be used to provide privacy, control circulation, provide security etc. The following guidelines for fencing shall be followed:

- *Fences located adjacent to public streets must be visually transparent, such as wrought iron railings or tubular steel fencing, and powder coated an appropriate color to complement adjacent buildings.*
- *Fences shall be stepped down rather than sloping with the grade.*
- *Wire fences constructed of industrial type materials such as chain link fencing are not permitted when located adjacent to public streets or parking areas.*
- *Walls shall be detailed with reveals, caps, overhangs, or other added visual interest.*
- *Walls shall be level or stepped rather than sloped with the grade. Walls with a finished face of flat poured concrete or CMU are not allowed when located adjacent to public streets, and shall have a veneer complimentary to adjacent buildings.*
- *Colors, design and materials of all fencing shall be complimentary to adjacent buildings and the landscape context.*
- *Refer to the section on Service Areas for further guidelines.*
- *Fence materials shall consist of vinyl, cedar, redwood, stone, etc., and shall comply with local ordinances.*



Transparent Fence with Detailed Wall

### 03.4.13 Maintenance

The property owner, homeowner or business association shall be responsible for the maintenance of all landscape areas. Landscaping shall be continually maintained including irrigation, weeding, pruning and replacing plant material or irrigation components as approved in the original design. The following standards shall apply to all landscape areas and materials:

- *Any replacement or alteration of plant material must be approved according to SWITC development protocol.*
- *Non-living ground covers, such as rock or organic mulch, must have 100% ground surface coverage and be maintained at the required depth.*
- *All plant material including trees, shrubs, groundcovers, vines and turf must have a 100% ongoing survival rate.*
- *Any dead or severely damaged plant material shall be replaced by the owner within six months of notification by the appropriate entity administering SWITC development guidelines.*
- *Pruning of plant materials shall not drastically alter the natural growth pattern and maturing size. To ensure the design intent is effective, all shrubs, perennials and groundcover should be maintained to the height and width as specified in the species selection lists for SWITC.*
- *Tree pruning within the right-of-way is allowed by permit*



Cedar Fencing

*only and must be performed by a City approved contractor. Topping is expressly prohibited. If the City determines that pruning has occurred that violates this requirement, the owner will be required to replace the affected plant with an equal plant within six months of notification by the City. This requirement also applies to plant material affected by storm damage.*

- *Plants infected with insects or disease must be treated appropriately or removed from the property, as required by the City. Plants removed must be replaced with equal plant materials.*
- *Weeds must be abated and removed.*
- *Tree grates in sidewalks and plazas shall be widened to accommodate the growing tree trunk and prevent girdling of any trees planted in tree wells.*
- *Turf areas shall be mowed, aerated, de-thatched, fertilized, and have weeds removed to ensure they are kept in a healthy condition.*
- *Irrigation is discouraged in the heat of the day (between the hours of 10 a.m. and 6 p.m.) in order to reduce evaporation. Excessive water runoff onto paved areas is not permitted.*
- *Irrigation systems shall be maintained and periodically adjusted to assure watering is efficient and conservation methods are effective. Replacement parts shall match or be compatible with the system requirements.*



Redwood Fencing



Vinyl Fencing

# 03 DESIGN DOCUMENTS

## 03.5 GOLF COURSE DESIGN GUIDELINES

### 03.5.1 Centennial Ridge Golf Course Overview

As an integral part of the SWITC Conceptual Master Plan, goals and objectives of the new golf course routing and design include:

- *Provide a dynamic public recreational use amenity within the development.*
  - *While data supports a reduction in the amount of golf on the property, it is intended to remain a key component of the new master plan.*
- *Compliment the quality of the other development components.*
  - *The golf course must be designed, constructed and maintained at a level that compliments the quality of the proposed development.*
- *Take advantage of existing natural features or areas of the property that lend itself to the design of the golf holes.*
  - *Areas of the property that are particularly suited for golf hole design will be used to create a unique golfing experience. Areas are sought out and used within the routing of the golf course to provide the greatest possible value. This will result in a fun and memorable golfing experience.*
- *Add premium values to adjacent properties.*
  - *Golf will be used to increase premiums on adjacent uses including housing, commercial, and retail/entertainment uses where possible. This will add considerable value to the development by adding premiums where they may not otherwise exist.*
- *Use the golf course as a buffer.*
  - *Golf can be used to separate the various uses on the property or to buffer against existing constraints or elements such as transmission lines, railways, and neighboring industrial uses.*
- *Route golf holes in areas that are not necessarily suited for other uses.*
  - *Areas of significant terrain and slope, within easements and adjacent to industrial uses are best suited for golf.*
- *Create a superior customer experience and amenities.*
  - *Additional customer amenities include an extensive and dynamic practice facility with sheltered and*

*heated hitting bays, pitching and chipping areas, an area at the clubhouse for outside group and catered events overlooking golf and mountain views, and a dedicated entry.*

- *Take advantage of the views and vistas to the north and west.*
  - *The Clubhouse will be positioned on high ground overlooking holes routed on the lower areas of the property with unobstructed views of the foothills and mountains to the north and west.*
- *Consider phased development of the new master plan.*

### 03.5.2 Components of the New Facility

- *New 18 hole championship caliber golf course routed over portions of the existing golf courses.*
- *Extensive golf academy practice facility including:*
  - *A range tee to accommodate up to 42 golfer stations with a portion covered and climate controlled.*
  - *An artificial turf strip along the back.*
  - *Realistic target greens within the range at various yardages.*
  - *300+ yard length*
  - *Cart and ADA access*
  - *Pitching and Chipping complex*
  - *Practice putting green*
  - *Area for teaching facility building*
  - *No required netting for ball containment*
- *Relocated and improved clubhouse facility*
- *Relocated turf management facility*
- *Potential 6 hole academy/short course*

### 03.5.3 Traits of the Golf Course Design

- *A wide range of course set up and yardage options ranging from 4,800 yards to 6,900 yards on 5 sets of tees and an additional championship/tournament tee configuration of 7,200 yards for designated use only.*
- *A good distribution of hole lengths, direction and shape.*
- *Strategically positioned bunkering throughout the holes that*

will provide interest to the round for all players and an appropriate challenge for better players.

- Wide and accommodating fairways for maximum playability.
- Large green surfaces for multiple pin locations and spreading of wear built to USGA specifications.
- Maintain walkability.
- Existing steep slope is incorporated into the design of several of the golf holes for added drama and interest.
- Areas outside of maintained turf improved to contain low water use grasses and plant materials.
- Concrete cart paths at all greens and tees only.
- Re-use of existing lakes, expansion and addition of two additional lakes.
- ADA access to all areas.
- Course developed to Audubon certification guidelines.

<i>Yardages and Par</i>						
Hole	Par	Black	Blue	White	Green	Red
1	4	400	385	370	335	295
2	5	530	510	490	453	415
3	4	370	346	315	290	240
4	4	405	380	352	328	285
5	3	162	152	135	128	112
6	4	465	425	406	380	348
7	5	515	500	486	452	400
8	3	150	138	132	108	98
9	4	428	406	378	340	318
<b>Out</b>	<b>36</b>	<b>3425</b>	<b>3242</b>	<b>3064</b>	<b>2814</b>	<b>2511</b>
10	4	356	342	323	300	265
11	4	368	350	332	305	275
12	3	216	196	175	160	138
13	5	505	475	450	420	375
14	3	200	178	160	151	126
15	4	432	410	395	358	325
16	5	561	528	500	478	430
17	4	385	360	340	290	262
18	4	435	392	368	325	310
<b>In</b>	<b>36</b>	<b>3458</b>	<b>3231</b>	<b>3043</b>	<b>2787</b>	<b>2506</b>
<b>Total</b>	<b>72</b>	<b>6883</b>	<b>6473</b>	<b>6107</b>	<b>5601</b>	<b>5017</b>

Centennial Ridge Score Card

# 03 DESIGN DOCUMENTS

## 03.5 GOLF COURSE DESIGN GUIDELINES

### 03.5.4 Hole-by-Hole Description of the Conceptual Routing

- *Hole 1 - A mid length par 4 with a dramatic tee shot down the hill to starts the round. An option to carry the hazards on the inside will provide the desired approach position for the 2nd.*
- *Hole 2 - Par 5 that plays backwards over the existing 14th hole of the Centennial course. The existing unattractive ponds are re-built into one large water hazard along the 2nd and 3rd legs that also buffers and adds premium to the new housing to the south. This results in a memorable risk reward 2nd shot to a green perched out over the water.*
- *Hole 3 - The tee shot on the short par 4 is played over the water with an optional tee location having a more challenging angle and longer carry to the fairway. The green is position between the canal and railway near the green of existing hole 12.*
- *Hole 4 - A par 4 that is routed over previous hole 9 newly graded for improved sight lines and a new green location back and right above the area of the removed old maintenance facility. The hole adds premium to the new development along the east side of the hole.*
- *Hole 5- The par 3 7th hole of Centennial is redesigned to maintain the dramatic shot over the canal but to an improved green complex suited for the shot. A larger tee complex provides more variety in set-up and angles to the green.*
- *Hole 6 - The awkward 8th hole of Centennial is used to create this dynamic par 4 with a split fairway option off the tee. Golfers can choose to play to the high ground on the right away from the canal where a more challenging 2nd awaits or down the lower left side which is guarded by the canal. Removal of the existing tee complex above allows for an improved green here which greatly enhances playability for all.*
- *Hole 7 - To take advantage of the existing dramatic slope this par 5 is routed beautifully along the top over the alignment of the existing 11th hole in reverse fashion. The green is perched along the edge creating a risk reward option on the 2nd shot. Conservative play is along the right.*
- *Hole 8 - The shortest par 3 on the course plays due north with a green situated along the top of the slope that drops down along the left. Bunkers flanking the long green establish interesting pin positions that will create different shot values on a daily basis.*
- *Hole 9 - Returning to the clubhouse this par 4 doglegs left along the sweeping ground to a green situated in the location of the Ridgecrest existing 18th tees. Fairway and greenside bunkering creates strategic and visual interest to the hole while the wide fairway maintains playability.*
- *Hole 10 - This shorter par 4 runs to the east between the new practice range on the left and future commercial or retail development on the right. A well contoured green surface provides interest to the hole that can be watched from the development.*
- *Hole 11 - The par 4 sweeps around a large lake on the inside to a cape green. Golfers are faced with a fun choice in determining their angle of carry over the water on their drive. Commercial development across the lake is afforded an intimate view of the hole for added premium.*
- *Hole 12 - The existing terrain along the top of the slope is taken advantage of here to create a dramatic par 3. The longest of the par 3's the hole can extend out to 230 yards for championship play where the carry over the slope is also greater to the green that is perched on the peninsula beyond. Uninterrupted views of the mountain ranges beyond will help make this a memorable golf hole.*
- *Hole 13 - The existing Ridgecrest 11th hole is enhanced at the green and fairway bunkering is added to compliment the fairway which is routed along the top of the slope. Housing added along the left is situated at a safe distance away from the hole.*
- *Hole 14 - The existing Ridgecrest 12th hole is enhanced with improved tee configuration and cart path that safely traverses the slope down to the green below. The area surrounding the green is also enhanced.*
- *Hole 15 - The existing Ridgecrest 13th hole is extended to a long par 4. The tee shot is played along the existing lake on the right as the hole dog-legs around the water.*
- *Hole 16 - This par 5 starts off with a split fairway option on the drive where players can choose to play across the canal where an easier 2nd shot awaits. Choices*



- abound on this hole but playability is maintained with the wide areas of play and conservative routes maintained. The canal comes into play again with the green situated along its bank near existing Hole 16 green.
- Hole 17 -** Routed over currently unused ground this mid length par 4 plays from elevated tees positioned on the hillside to a wide fairway guarded only at the farthest end. The green is designed to require a well placed approach to post a low score.
- Hole 18 -** This dramatic par 4 finishing hole plays over beautiful natural terrain that is particularly suited for golf. Perched along the slope golfers will pick their angle of carry off the various tees to gain the greatest advantage. Approach shots are played to a green that is perched out on the tip of the peninsula.
- Clubhouse -** Arrival to the Clubhouse extends beyond the existing transmission and power lines which then no longer interrupt the beautiful vistas and views from the setting. The building is situated to overlook the dramatic finishing hole as well as play down the 1st hole.

Conceptual Master Plan Showing the Proposed Centennial Ridge Golf Course

Not to Scale

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

### 03.6.1 Introduction

The architectural character acknowledges that all ground in the Southwest Idaho Treatment Center Master Plan represents an important master planned development within the city of Nampa. Within the planned area the intended character varies relative to the identified uses. It is important that the architectural character of each different building type be interesting, diverse, innovative, thoughtful, and compatible within the property boundaries, and that it meet the provisions contained in these design guidelines. The architectural character encourages variations of form, massing, and scale of the buildings, and the incorporation of high quality and appropriate building materials such as stone, brick, stucco, window and storefront glass systems, roofing, canopies, and building lighting. In addition, all structures will be responsive to the site location and amenities. Ultimately, the design intent of the guidelines is to foster creative, interesting, cohesive and timeless architectural and landscape designs throughout the Southwest Idaho Treatment Center.

The architectural design guidelines provide an aesthetic framework under which physical structures are designed and incorporated into the site. The guidelines address site placement in conjunction with the Site Design Guidelines, building form, materials, and finishes. The Architectural Design Guidelines include specific provisions for the building types identified below:

- Commercial Buildings including office, technology and medical campus development
- Mixed-use Commercial Buildings with a mix of commercial, retail, commercial office, and multi-family residential.
- Residential Buildings including single family detached, single family attached, multi-family condominiums, townhouses, and apartments.

### 03.6.2 Commercial Buildings

This section represents commercial buildings allowed in specific zones independent of mixed-use and or residential zones as defined by City code, and the Southwest Idaho Treatment Center Master Plan. Commercial buildings are intentionally located on the master plan concepts to inform the desired site character, and may include one, two, three, and four story structures that are commercially oriented in uses. Allowable Commercial uses will be identified.

These design guidelines will address the opportunities of form, building height, imagery, proportion, and elements of building structures. The building elements include entrances and storefronts, windows and doors, roofs, materials and colors, exterior lighting, building signage, and site relationship among others.

#### FOBM

Architectural form in commercial building types should be varied based on location use, and the uses of the physical adjacencies. Buildings identified as strictly commercial office are most likely located within low density business communities, and will be stand alone structures exposed on all elevations. These buildings may be two to four stories, with articulation of facades and rooflines.

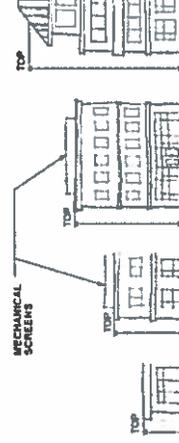
#### HIGHEST

Building height within the Southwest Idaho Treatment Center Master Plan area will be limited by the Master Plan and shall reference Nampa City Code. All of the commercial structures shall comply with height limits specific to the zoning area. Commercial zone maximum height limits may vary from thirty feet to fifty feet. Measurement of building height is per the Nampa City Zoning Ordinance Business Districts:

#### Height - Buildings:

The vertical distance from the grade to the highest point of the coping of a flat roof or to the deck line of a mansard roof or the average height of the highest gable of a pitch or hip roof.

The ordinance recognizes the challenge with mechanical screening and has exempted it from the height measurement when determining the building height.



Measurement Diagram for 1-4 Story Buildings

**IMAGERY**

The aesthetic imagery of commercial structures in the SWITC Master Plan project are based on building size, height, form, materiality, and composition of all visual building elements. All of these components are expected to develop the imagery of the structure as a three dimensional form within the environment. The design guidelines are written such that the building designer is motivated to use all the tools available to create a structure of interesting design, using high quality materials, and in a fashion that speaks to permanence, longevity, lasting quality, and is contextually responsive to the surrounding development and structures.



*1-Story Commercial/Retail Building*



*2-Story Commercial/Retail Building*



*3-Story Commercial/Retail Building*



*2-Story Commercial/Retail Building*



*Hotel*



*4-Story Commercial/Retail Building*



*Bank*



*Bank*

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES



Proportion Diagram

### PROPORTIONS

Buildings may be of a scale and proportion that relates well to adjacent buildings without dominating, overwhelming or appearing insubstantial in relationship. Long walls may be relieved with offsets, balconies, projections, recesses, or other architectural features. The façade of buildings shall be articulated into architecturally-distinct sections. Articulation can be achieved by change in plane, material, color, or a combination thereof.

The composition of elements should recognize the human scale through material changes, and or articulation within the composition, and building form modulations. Facades are encouraged to incorporate visually continuous details. These may be interrupted by windows and doors, as well as form modulations.

### ARCHITECTURAL ELEMENTS, COMPONENTS AND DETAILS

#### 01 Entrances and Storefronts

Entrances and storefronts typically identify the entrance to a building and define major from minor elevations. They represent a dynamic tool and element in defining primary and secondary access points as well as set the tone for a structure. Similar to windows, they are infinitely flexible in configuration giving the designer limitless opportunities in composition. Designed to be welcoming and at a human scale, while lending proportion and size to a building's composition. They can enhance building modulation by integration in a design at all levels and applications.

Storefronts shall include the following elements:

- *Large storefront windows.*
- *Doors easily identifiable.*
- *Visually attractive.*
- *Visually strong within composition.*
- *Awning or canopy at entrance.*



Entry Imagery



- *Horizontal human scale element(s).*

Other pedestrian friendly elements are encouraged:

- *Pedestrian scale signage.*
- *Planter boxes and hanging baskets.*
- *Public art such as sculpture, murals, or water features.*
- *Seating such as chairs or benches.*

Entries to office or reception areas for individual uses shall be on the front or approach side of the building, shall be easily visible and distinguishable from adjacent parking areas through the use of architectural elements, and shall not rely on disproportionately scaled signage.

### 02 Canopies, Trellises and Awnings

Canopies, trellises, and awnings serve a number of purposes both functionally and aesthetically. They provide coverage from inclement weather, shade from sun, and if done well, can enhance the compositional aesthetics of a building. They can be constructed of permanent materials or temporary fabrics. In addition to protection they can be transparent or translucent, enhancing the experience of the pedestrian as they engage the canopies.

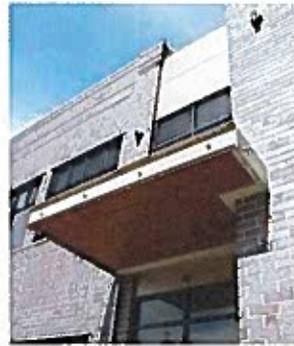
Trellises are also an element that provide protection from the sun, create a framework on which to grow plants and vines, and are another tool to enhance a building's aesthetic. Trellises can also be used as a landscape element fully independent of any building. Material can be metal, wood, or vinyl. Finishes can vary depending upon the desired look. Trellises can be placed at the ground level or on upper floors, decks, and walls.

### 03 Windows and Doors

Windows and doors – similar to storefronts and entrances, provide a strong design element that when applied properly reinforces the building composition and aesthetic. Both doors and windows can establish a patterned rhythm within a building. Placement within a wall can create the impression of depth and solidity. Placed at the face of a building's surface material, the imagery is much different. Placement is very important in the composition.

Doors identify commerce and entry, and should be highly visible and easily legible by the public. The sense of entrance should be reinforced by the composition of an entry system, placement of the doors, specific style and material.

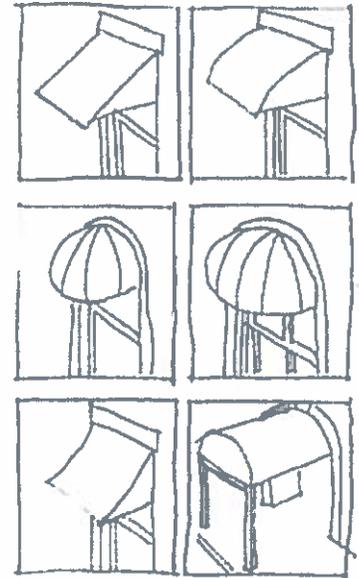
Windows come in all sizes and shapes, materials, and colors,



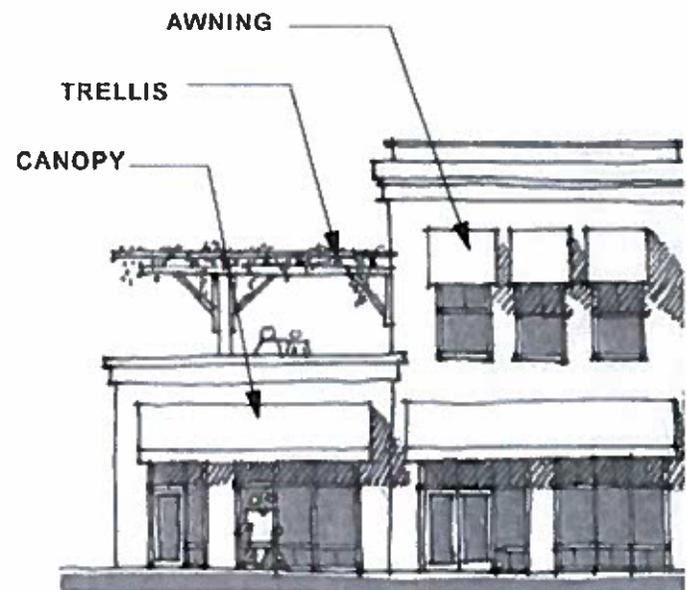
Canopy



Trellis



Awning Diagram



# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

as well as operability, transparency, and ability to join in configurations limited by design. Shapes include ribbon windows in aluminum frames, simple divided windows in single openings, to ganged groupings of two or more windows “mulled” together to create a larger assembly. Selection of window type and style, as well as material must enhance the overall composition of the building.

Openings shall be appropriate to the general aesthetic of the office building, and may include individual ‘punched’ windows, or groups of openings, ribbon windows or storefronts.

Entry doors may be recessed to create a modulated ‘street wall’ and create an interesting contrast of shade and shadow.

Entry assemblies and doors may be celebrated with contrasting colors, high quality materials, and surrounds to call attention to building entrances.

Windows shall be of differing sizes reflecting the various public or private rooms within.

Ground level windows shall extend above an 18 to 24 inch base.

Recessing and trimming of doors and windows is highly encouraged to create shade and shadow across the face of the building.

Doors and windows should be consistent throughout comparable locations on/in the building.

To create activity on the sidewalk special attention should be given to windows and doors as they offer activity and visual excitement to the commercial areas. Doors are encouraged every 30’ along storefront buildings

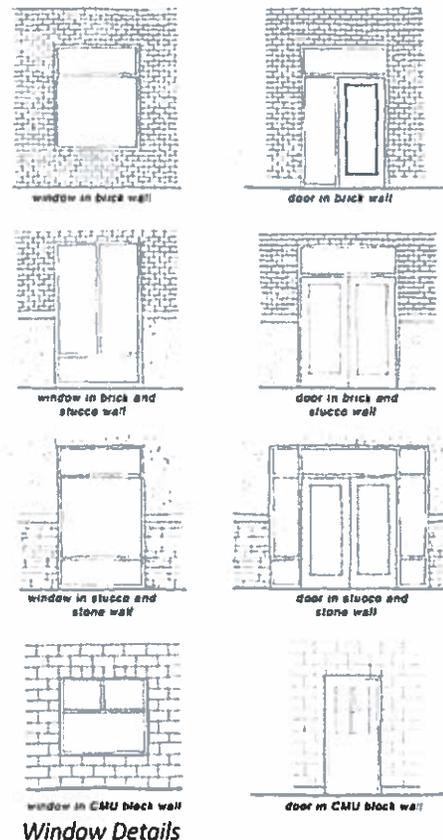
Window shading devices are highly encouraged for summer sun protection.

Door / window compositions should be compatible with all other opening installations in a building design.

### 04 Exterior Lighting

Exterior lighting is intended to serve three purposes-enhance safety, conserve the ‘dark sky,’ and enhance the building aesthetic.

- *Low intensity light sources shall be used with frosted or translucent lenses and ‘cut-off’ fixtures.*
- *Light sources shall not be visible off-site.*
- *Up lighting of trees, vegetation, buildings, outbuildings, and landscape structures is allowed.*



Window Details

- *Holiday lights are allowed for seasonal celebrations.*

### 05 Materials and Colors

Materials for buildings at SWITC should reflect the heritage of the site and include local materials found in the vicinity. These include a combination of enduring materials such as limestone, sandstone, granite, basalt, stucco and brick as the dominant exterior wall material.

Accent materials such as wood and fiber cement paneling, stucco, and formed metal can be integrated into the overall design to add visual interest, enhance scale, and support the overall design composition. Where material changes happen, they must occur at a clear break in the surface plane of the building. Materials shall be consistently applied to all elevations and shall generally wrap corners prior to a transition.

Undersides of decks, balconies, bay windows, soffits, and other elements, if visible from below, shall be finished consistently with adjacent level of finish. Similarly, topsides of such elements shall also be finished with consistent high level finishes.

Large areas of color shall mimic tones which exist in the natural landscape. Deep colors may be used as accents. Colors should celebrate and differentiate commercial buildings from each other and be consistently applied.

#### 06 Signage

Building signage is important for the identity of the buildings and should be integrated into the design and order of all structures. To create an interesting pedestrian street scene, creative and lively signs are highly encouraged.

All signage must be submitted for review with the building design review submission.

#### 07 Roofs

The integration of visible roofs as part of a building design and composition provides the opportunity to enhance a structure's design through additional visual interest, modulation, and articulation. On high bay buildings, the use of sloping roofs around the perimeter, within or exceeding the depth of the structural and mechanical systems, are encouraged as a means of visually reducing the scale when appropriate.

Roofs may be either flat or sloped, and must be made of durable, quality materials consistent with first class commercial construction. Sloped roofs must use long lasting materials such as natural slates, ceramic tile, concrete tile, architectural grade composition shingles, or seamed metal roofing materials. Other products will be considered upon full review of the material and its proposed integration into the design.

Roof terraces and gardens are encouraged and may include pavilions, pergolas, trellises and other enlivening structures made of compatible materials.

Parapets and cornices are required for flat roofs. Parapets must be capped and may be stepped. Decorative elements of parapets

may exceed height limits by 4 feet maximum.

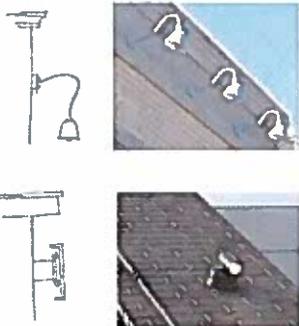
Gutters are required where roofs are exposed to view. In such case, downspouts and rain water leaders will be required to contain the flow of runoff from the roofs down to a grade collection system. Water will not be allowed to flow across patios, sidewalks, ramps, parking areas or roads. Downspouts should be handled as a component of the design composition and be located in logical locations relative to the overall building design, and site storm water facilities.

#### 08 Miscellaneous (Dumpsters, Satellite Dishes, Mechanical Equipment, Etc.)

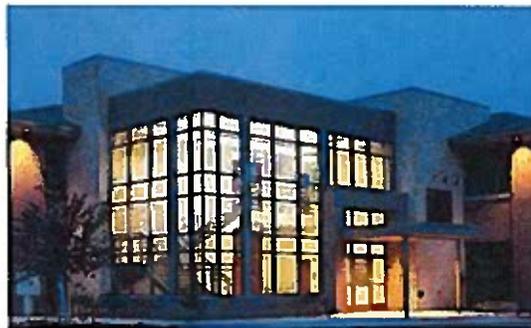
Dumpsters, trash disposal equipment, mechanical equipment, meters, satellite dishes and exterior work areas remain necessary components of most structures and should be considered with the same standard and care as the main structures. These support elements must be completely screened from view within enclosed yards responding to the design of the adjacent structure they are supporting.

Elements such as dumpsters must have hinged tops to prevent unsightliness, blowing of trash around the site, and wildlife access. Satellite dishes and other communication equipment must also be screened from public view. Screening can be addressed by on ground enclosures or roof area enclosures. Satellite dishes may be no larger than 36" in diameter to avoid the necessity of disproportionately tall enclosures.

Loading docks and areas supporting buildings shall be located such that the function is well screened from view of adjacent structures and pedestrians. Truck parking in support of commercial buildings, large doors and loading/unloading areas shall be located within walled courts, wings of the building, or a combination of both to substantially conceal the activity. Such structures including gates, grilles and fencing, must be designed to tie into the aesthetic of the building they are connected with.



*Lighting Details and Imagery*



# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

### 03.6.3 Mixed-Use Buildings

Mixed-Use commercial buildings may include one, two, three, and four story structures that are a mix of commercial and residential oriented uses. These uses include office, retail, banks, shops, and restaurants. Within the guidelines we address the opportunities of form, building height, imagery, proportion, and elements of building structures. The elements include entrances and storefronts, windows and doors, roofs, materials and colors, exterior lighting, building signage, and site relationship.

#### FORM

Architectural form in the mixed-use commercial building type is varied based on location within the SWITC plan area, the intended tenant uses, and the adjacencies on site. Buildings identified as mixed-use buildings are most likely located adjacent to the central access road and will commonly be multi-building structures with exposure on two or three elevations. These buildings are intended to be one to three stories, with articulation of facades that may include stepping in plan and elevation.

The goal of the mixed-use commercial is to create an urban feel by the establishment of a vibrant street façade with variation in architectural style and tenant use.

Buildings shall be of a scale and proportion that relates well to adjacent buildings without dominating, overwhelming or appearing insubstantial in relationship. Long walls shall be relieved with offsets, balconies, projections, recesses, or other architectural features.

To maintain pedestrian friendliness, building forms must be articulated with a base, middle, and top.

- *Top elements define roof or parapet with a distinct three dimensional outline or profile achieved with projections such as cornices, canopies, parapets, or pitched roof eaves with changes in materials and colors.*
- *Middle elements must be distinct in material and color from the top and base. Window articulation may vary from the base and top in support of the composition.*
- *Base elements must be 5' tall at a minimum, (preferably closer to a full story in height), and must be made with an enduring material such as stone or brick, or combination thereof.*
- *Corner elements may be celebrated in forms such as towers, projecting bays, and balconies. Balconies, step backs, and other forms are encouraged for visual variety.*

#### HEIGHT

Building heights will be limited by the Southwest Idaho Treatment Center Master Plan Zoning Ordinance. All of the mixed-use commercial structures will be held to the height limits specific to the zoning area.

Measurement of building height is per the Nampa City Zoning Ordinance:

#### Height, Building:

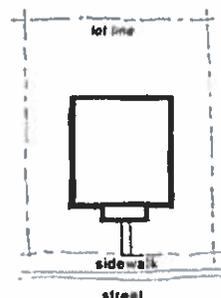
The vertical distance from the grade to the highest point of the coping of a flat roof or to the deck line of a mansard roof or the average height of the highest gable of a pitch or hip roof.

The ordinance recognizes the challenge with mechanical screening and has exempted it from the height measurement when determining the building height.

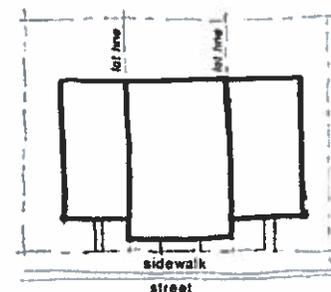
#### PROPORTIONS

Buildings shall be of a scale and proportion that relate well to adjacent buildings without dominating, overwhelming or appearing unrelated. Long walls shall be relieved with offsets, balconies, projections, recesses, or other architectural features. The façade of buildings shall be articulated into architecturally-distinct sections with each section taller than it is wide. Articulation must be by change in plane, material or color.

To create a human scale, facades are encouraged to incorporate visually continuous details, these may be interrupted by windows and doors. *Note: Building Proportions Diagram references proportional distances at the elevation plane.*



**standalone building**  
Footprint Articulation



**zero lot line buildings**

**IMAGERY**

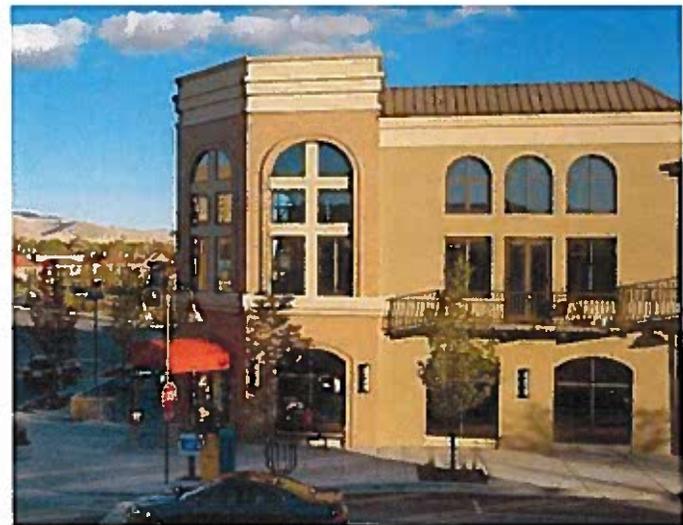
The imagery for mixed-use commercial structures in the Southwest Idaho Treatment Center Master Plan project is not defined as a prescriptive requirement. Building size, height and form are expected to inform the imagery of the structure as a three dimensional form in the environment. Additional imagery components include building materials, architectural elements, and the composition of these features. The Southwest Idaho Treatment Center Master Plan is written such that the building designer is motivated to use all the tools available to create a structure of interesting design; with high quality materials; in a fashion that speaks to permanence, longevity, lasting quality; and is contextually responsive to the surrounding development and structures.



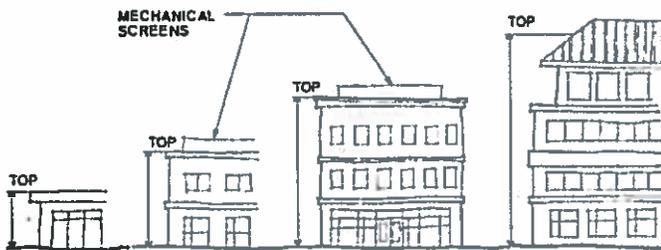
*2-Story Commercial/Retail Building*



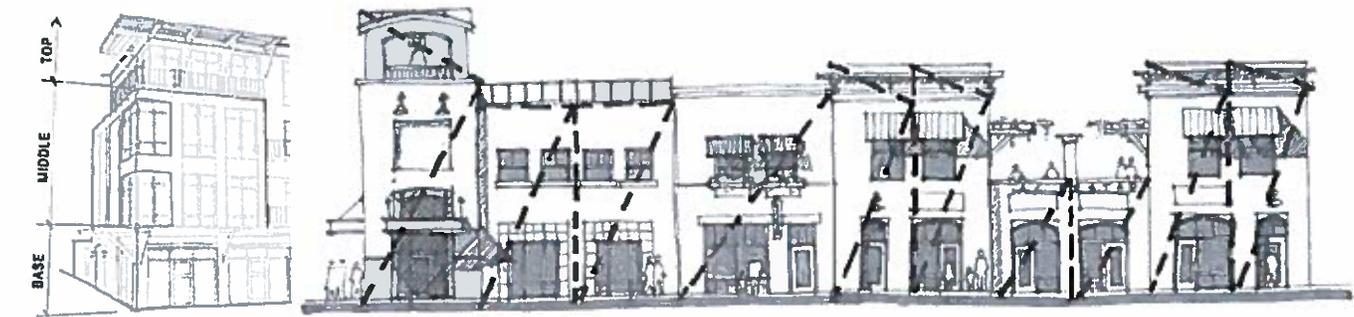
*1-Story Commercial/Retail Building*



*3-Story Commercial/Retail Building*



*Measurement Diagram for 1-4 Story Buildings*



*Form Articulation*

*Building Proportions Diagram*

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

### ARCHITECTURAL ELEMENTS, COMPONENTS AND DETAILS

#### 01 Entrances and Storefronts

In mixed-use building design, entrances and storefront are critical to the success of creating a space that meets the necessities of the tenant relative to identification, ease of discovery, and in the case of retail, visual access to the space. Entries must be oriented to the street, and easily identifiable. A protective covering is encouraged such as a canopy, trellis, or portico. Other additional enhancement should be provided near the entrance such as lighting, seating, ornamental potted plantings, and possibly a special paving pattern announcing the entry.

Storefronts should further enhance the retail function of the space behind and may include the following elements:

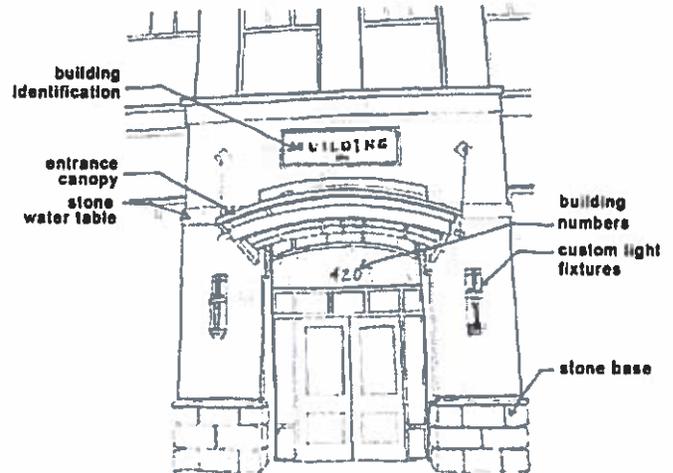
- *Large, raised storefront windows with projecting sills.*
- *Mullions that provide for ease of visual access to displays.*
- *Storefront compositions that provide flexibility for varying types of retail tenants beyond.*
- *Storefront configurations that enhance the architectural design of the building while addressing proportion, pedestrian scale, and comfort when standing adjacent to it.*
- *Horizontal human scale element (see Proportion guidelines).*

Other pedestrian friendly elements are encouraged:

- *Planter boxes and hanging baskets.*
- *Awning or canopy at entrance and along the storefront openings.*
- *Structural and architectural articulation between storefronts.*
- *Public art such as sculpture or murals.*
- *Seating such as chairs or benches.*



Entrances and Storefronts



Entry Detailing

#### 02 Canopies, Trellises and Awnings

Canopies, trellises, and awnings serve a number of purposes both functionally and aesthetically. They provide coverage from inclement weather, shade from sun, and if done well, can enhance the compositional aesthetic of a building.

They can be constructed of permanent materials or temporary fabrics. In addition to protection they can be transparent or translucent, enhancing the experience of the pedestrian as they engage the canopies.

Trellises are also an element that provide protection from the sun, create a framework from which to grow plants and vines, and is another tool to enhance a building's aesthetic. Trellises can also be used as a landscape element fully independent of any building. Material can be metal to wood to vinyl. Finishes can vary depending upon the aesthetic desired. Trellises can be placed at the ground level or on upper floors, decks, and walls.



Trellis



Canopy

### 03 Windows and Doors

Windows and doors – similar to storefronts and entrances, provide a strong design element that applied properly reinforces the building composition and aesthetic. Both doors and windows can establish a patterned rhythm within a building. Placement within a wall can create the impression of depth and solidity. Placed at the face of a building's surface material, the imagery is much different. Placement is very important in the composition.

Doors identify commerce and entry, and should be highly visible and easily legible by the public. The sense of entrance should be reinforced by the composition of an entry system, placement of the doors, specific style and material.

Windows come in all sizes and shapes, materials, and colors, as well as operability, transparency, and ability to join in configurations limited by design. Shapes include ribbon windows in aluminum frames, simple divided windows in single openings, to ganged groupings of two or more windows "mulled" together to create a larger assembly. Selection of window type and style, as well as material must enhance the overall composition of the building.

Openings shall be appropriate to the general aesthetic of the building, and may include individual 'punched' windows, or groups of openings, ribbon windows or storefronts.

Entry doors may be recessed to create a modulated 'street wall' and create an interesting contrast of shade and shadow.

Doors shall be celebrated with contrasting colors, materials, and surrounds to call attention to building entrances.

Windows shall be of differing sizes reflecting the various public or private rooms within.

Recessing and trimming of doors and windows is highly encouraged to create shade and shadow across the face of the building.

Doors and windows should be consistent throughout the building.

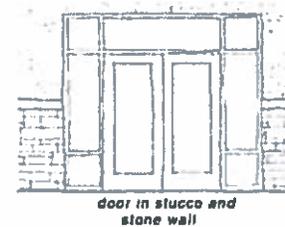
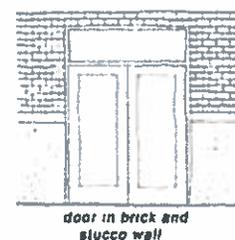
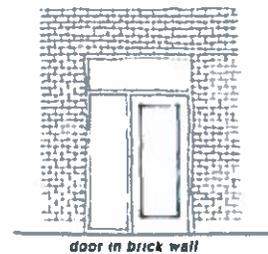
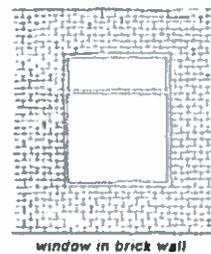
To create activity on the sidewalk, special attention should be given to windows and doors as they offer activity and visual excitement to the commercial areas of the Southwest Idaho Treatment Center Master Plan area, and are highly encouraged to have active doors every 30' along storefront buildings

Window shading devices are highly encouraged for summer sun protection.

Vertically rectangular windows should dominate. Shallow arched windows are allowed in masonry openings.

Ground level windows shall extend above an 18 to 24 inch base.

Glazing may extend from the head to the ground or paving surface.



#### Window Details

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

### 04 Exterior Lighting

Exterior lighting is intended to serve three purposes—enhance safety, conserve the ‘dark sky,’ and create a cohesive pedestrian identity. Within the mixed-use commercial area of the SWITC development, lighting is also important to promote the businesses. Tenant spaces should be marked with interesting lighting complimentary to the building composition. Where buildings are combined, lighting should be evaluated and the appropriate fixtures and type should respond to the architecture. It is not necessary that one single fixture be used on all buildings.

Lighting should be treated as a design element within the overall building composition. Items to consider when selecting exterior building lighting include:

- *Light sources shall not be visible off-site nor distracting at the source.*
- *Low intensity light sources shall be used with frosted or translucent lenses and ‘cut-off’ fixtures.*
- *Up lighting of canopies, trees, and building elements to be done in a fashion not distracting of other aspects of the design.*
- *Average lighting levels shall not exceed 15 foot candles measured at the frontage of buildings.*
- *Holiday lights are allowed for seasonal celebrations.*

### 05 Materials and Colors

Materials for buildings at Southwest Idaho Treatment Center Master Plan should reflect the heritage of the site and include local materials found in the vicinity. These include a combination of enduring materials such as limestone, sandstone, granite, basalt, stucco, and brick as the dominant exterior wall material.

Accent materials include wood and fiber cement paneling, stucco, and formed metal can be integrated into the overall design to add visual interest, enhance scale, and support the overall design composition. Where material changes happen, they must occur at a clear break in the surface plane of the building. Materials shall be consistently applied to all elevations and shall generally wrap corners prior to a transition.

Undersides of decks, balconies, bay windows, etc. if visible from below, shall be finished consistently with adjacent level of finish. Similarly, topsides of such elements shall also be finished with consistent high level finishes.

Large areas of color shall reflect the neighboring natural landscape and natural materials. Deep colors may be used as accents. Colors should celebrate and differentiate homes and commercial buildings from each other and be consistently applied.

### 06 Signage

Building signage is important for the identity of tenants within the mixed use core of the SWITC plan and should be integrated into the design of the building. To create an interesting pedestrian street scene, creative and lively signs are highly encouraged. Refer to the guidelines for project and building signage later in this document.

All signage must be submitted for review with the building design review submission.

### 07 Roofs

The integration of visible roofs as part of a building design and composition provides the opportunity to enhance the structures design through additional visual interest, modulation, and articulation. On high bay buildings, the use of sloping roofs around the perimeter, within or exceeding the depth of the structural and mechanical systems, are encouraged as a means of visually reducing the scale when appropriate.

Roofs may be either flat or sloped, and must be made of durable, quality materials consistent with first class commercial construction. Sloped roofs must use long lasting materials such as natural slates, ceramic tile, concrete tile, architectural grade composition shingles, or seamed metal roofing materials. Other products will be considered upon full review of the material and its proposed integration into the design.

Roof terraces and gardens are encouraged and may include pavilions, pergolas, trellises and other enlivening structures made of compatible materials.

Parapets and cornices are required for flat roofs. Parapets must be capped and may be stepped. Decorative elements of parapets may exceed height limits by four (4) feet maximum.

Gutters are required where roofs are exposed to view. In such case, downspouts and rain water leaders will be required to contain the flow of runoff from the roofs down to a grade collection system. Water will not be allowed to flow across patios,

sidewalks, ramps, parking areas or roads. Downspouts should be handled as a component of the design composition and be located in logical locations relative to the overall building design.

#### 08 Parking

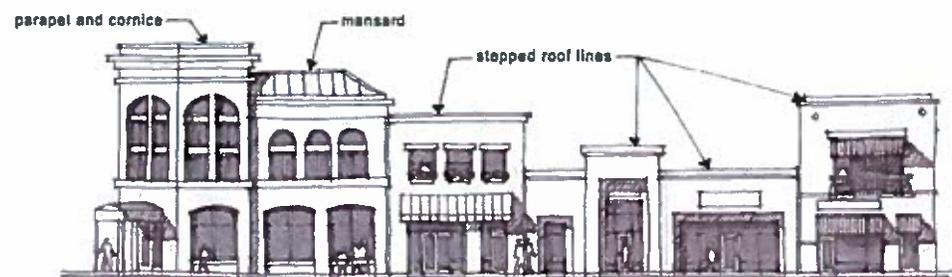
Refer to landscape guidelines for parking design.

#### 09 Miscellaneous (Dumpster, Satellite Dishes, Mechanical Equipment, Etc.)

Dumpsters, trash disposal equipment, mechanical equipment, meters, satellite dishes and exterior work areas remain necessary components of most structures and should be considered with the same standard and care as the main structures. These support elements must be completely screened from view within enclosed yards responding to the design of the adjacent structure they are supporting.

Elements such as dumpsters must have hinged tops to prevent unsightliness, blowing of trash around the site, as well as wildlife access. Satellite dishes along with other communication paraphernalia must also be screened from public view. Such screening can be within on ground enclosures or roof area enclosures. Satellite dishes may be no larger than 36" in diameter to avoid the necessity of disproportionately tall enclosures.

Loading docks and areas supporting buildings shall be located such that the function is well screened from view of adjacent structures and pedestrians. Truck parking in support of commercial buildings, large doors and loading/unloading areas shall be located within walled courts, wings of the building, or a combination of both to substantially conceal the activity. Such structures, including gates, grilles and fencing, must be designed to tie into the aesthetic of the building with which they are connected.



*Roof Detail Diagram*

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

### 03.6.4 Residential Buildings

The residential land use designations within the Southwest Idaho Treatment Center Master Plan offer numerous residential building opportunities ranging from single family structures and town-house residences to high density condominium living. The mix of residential product is anticipated to be varied in size, configuration, type, form, and aesthetic. As mentioned, the design guidelines anticipate structures ranging from traditional detached single family dwellings to multi storied condominium structures, with variations between including attached single family townhouses, twins, and quads, depending on the specific residential area. The important aspect of design and aesthetic within each residential planning area is a consistent push toward compatible imagery and quality, as well as a respect of existing context when it exists.

The residential designations within the Southwest Idaho Treatment Center Master Plan are defined relative to the ordinance criteria of density, type (single family / multi family), and massing including structure height and structure setbacks from property lines.

#### FORM

The variations of form within the different residential areas can be expressed as single residence structures, combined residential structures with similar elements to single structures, and larger structures expressing individual units within a greater composition. Generally, form for residential buildings is intended to be more intimate in character from that of commercial structures. In support of this approach, higher density dwelling structures may appear as large homes with a single entrance. In which case, individual units should be expressed with window groupings, porches, balconies, bay windows and other three dimensional articulation to break up mass. Town homes should be articulated to be different from each other by using bays, varying roof lines, and porches. The same can be said for detached single family residences when creating a residential community. Structures can be individual in character and imagery, while being a strong element in the fabric of the community.

#### HEIGHT

Designated within the Southwest Idaho Treatment Center Master Plan Zoning Ordinance, height is stated in terms of the maximum limits. Up to that point residential structures can set the building height at a constant, or vary through interesting forms and volumes.

As the residential unit density decreases, the height of residential

structures is reduced to be compatible with adjacent properties. Higher density structures will be placed in areas where additional height should incorporate varying roof lines and heights to maintain the sense of residential design.

Measurement of building height is per the Nampa City Zoning Ordinance:

#### Height, Building:

*The vertical distance from the grade to the highest point of the coping of a flat roof or to the deck line of a mansard roof or the average height of the highest gable of a pitch or hip roof.*

The ordinance recognizes the challenge with mechanical screening in higher density residential units and has exempted it from the height measurement when determining the building height. Mechanical equipment screening and mechanical penthouses may exceed parapet heights by no more than five (5) feet in unoccupied spaces.



Building Height Diagram

#### TYPES AND STYLES

##### 01 Single Family Detached and Attached

Generally, all houses should reflect the building traditions of the region, which are based on Idaho's climate, indigenous materials, and craftsmanship, as well as historic periods of settlement and development.

Some examples of common design elements are buildings with deep overhangs, wall offsets, recessed windows and doors, dormers, and the use of straightforward natural materials.

Particular architectural vernaculars lending themselves to residential design include adaptations of the following styles:

- *Craftsman*
- *Prairie Style*
- *Shingle*
- *Modern*
- *Victorian*

While the design guidelines anticipate the adaptation of specific architectural vernaculars, it is the intent that single family structures be designed to blend into the specific planning areas with interest in design and materials.

### 02 Multi-Family- Medium Density

Medium density multifamily residential design provides opportunities to create the density within structures that look like larger residences. The adjacent image combines multiple units in a composition that is very residential in scale, provides the identity of individual entries, while affording the aesthetic of residential looking structures. Through the variation of roof forms, building height, window fenestration, and residential scale detailing, the building establishes a residential quality.

### 03 Multi-Family- High Density

High density residential structures embody many of the characteristics of a commercial building in response to the needs of the program. This building type can integrate into the Southwest Idaho Treatment Center Master Plan by establishing a residential feel in a structure that is considerably larger than single family homes or low to medium density residential design.

The integration of materials, the overall building design, and the detailing of elements within the composition can reinforce the residential nature of the building while supporting an aesthetic appropriate to the size and scale of the structure. As structures become larger in mass it is important that the pedestrian experience along the base of the building be a residential scale.

### BUILDING MASSING AND FORM

In general, building mass shall be residential in scale and should respond to the surrounding block, lot type and size in which the residence is located within the Southwest Idaho Treatment Center Master Plan areas. Building design shall incorporate varied projections and recesses, including bay windows, dormers, porches, etc. Elements such as these will create visual interest



*Single Family, Detached*



*Single Family, Attached*



*Multi-Family-Medium Density*



*Multi-Family-High Density*

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

and should respond to existing site conditions on each particular home site as well as the surrounding built and natural environment.

All residential buildings are to be designed and built with a similar material palette on all elevations, giving equal attention to the sides and rear elevations as is given to the street side elevation.

All residential buildings should be particularly sensitive to their street frontage. Design elements that create a play of light and shadow and reduce the perceived bulk such as deep porches, decks, overhangs, multi-paned windows and deep offsets should be used.

The use of detached garages and breezeways connecting to the main house are encouraged where block design permits.

Houses located on sloped sites shall respond to the topography and shall integrate the building into the existing landform through the integration of elements including day lighted basements, stepped plans and responsive landscape.

Asymmetrical compositions of residential building forms are preferred.

### WINDOWS, DOORS AND ENTRIES

The use of recessed doors (entrances as well as garage doors) and window openings is encouraged. This use will create shadow lines to give the house a more substantial appearance.

Entry elements shall be in scale with the relative proportions of the house and streetscape. Dominating and over stylized entries will not be accepted.

All openings shall appear as individual 'punched' windows, or groups of openings. Horizontal as well as vertical mullions are encouraged to reinforce residential scale; true divide lights are desired.

The shape and detail of all openings are to be appropriate to the style of architecture. Window styles are to be consistent throughout the entire building.

Glass and glazing may be coated or tinted to control solar heat gain. Mirrored glass is not permitted in any instance.

Double or triple pane windows are required.

Exterior finishes of all windows shall be wood, colorfast vinyl or bronze anodized (or other appropriate color) finish. Unfinished aluminum is not allowed.



*Building Massing and Form Imagery*

### ROOFS

From many viewpoints in and around the Southwest Idaho Treatment Center Master Plan community, roofs are a dominant element of the landscape and must create a harmonious relationship with the surrounding block, street, site and adjacent structures. All roofs shall be carefully designed in form, materials, and color so that they integrate the structure with its landscape, setting, and neighboring buildings. All roof materials shall be class 'A' fire rated and non-reflective.

Materials for roofs include, without limitation:

- Unglazed tile
- Slate
- Concrete tile
- Architectural shingles
- Non reflective metals.

Flat roofs for mechanical equipment shall be concealed by sloping roofs or portions of the building. Dormers for windows, louvers and vents are encouraged on large expanses of roof, and parapets, cornices, and other detailing is desirable. Rooftop equipment and large vents are to be grouped and fully concealed in chimney-like structures as integral parts of the roof and/or wall design and shall match the roof in color. Ridge vents are encouraged.

All skylights, solar equipment, antennas, dishes and other roof appurtenances will be reviewed on an individual basis by the Southwest Idaho Treatment Center Master Plan Design Review protocol.

Roof dormers and other three-dimensional elements should be used to add large-scale texture to roof forms, avoiding the

appearance of wide, unbroken roof planes. The use of large roof overhangs is strongly encouraged.

#### CHIMNEYS AND ROOF PROJECTIONS

All roof projections, including chimneys, flues and vents shall be compatible in scale, height, and material with the structure from which they project. Where possible, large vents are to be grouped and concealed in chimney-like structures as integral parts of the roof or wall design. All rooftop hardware shall be painted to match the roof color.

Chimney hardware must be fully screened within an architectural feature.

Chimneys on exterior walls must be integrated into the building design in order to anchor the building to the site.

#### PORCHES AND DECKS

The use of porches, patios, terraces and decks in building design is encouraged to create a strong relationship between indoor and outdoor areas, encouraging the creation of a sense of community.

Porches, verandas, colonnades, terraces, and patios for climate control, circulation, and outdoor living shall be designed as integral elements of the building and site.

Houses on corner lots shall incorporate front and side elements in the building design.

Minimum depth of porches shall be six feet.

Materials of these elements shall match or compliment those of the main structure.

#### RAILINGS

The use of railings on porches, balconies and upper level windows or door openings should be carefully considered as a component of an architectural style. When properly applied, well-designed and properly detailed railings are an opportunity to reinforce specific characteristics of the selected architectural style. The materials used for railings should be part of an appropriate palette of materials for the architectural style of the building.

#### MATERIALS AND COLORS

Exterior use of materials and colors provide the opportunity to create individual identity for each residence while also being responsive to the context of the community and adjacent structures. Following are a number of items that the designer may incorporate to enhance the residential unit designs be it single family or multi-family structures:

- *Exterior walls and finishes should reflect a logical and appropriate combination of colors, textures and forms to compliment the context of the surrounding built and natural environment.*
- *Exterior walls of all residential buildings may use a maximum of three materials with one being dominant over the others in a logical, structural relationship.*
- *When a change in the materials occurs, a clear break in the surface plane should be seen. Materials should be consistently applied to all elevations of the structure.*
- *All building facades must include a significant degree of texture such as that provided by the use of wood (or cement panel) shingles, shiplap, board and batten applied sidings, stone, and brick. No vinyl siding is allowed.*
- *Stucco may be used in small amounts and must be used in conjunction with at least one other material. Frequent control joints, significant textural qualities and color variations are required.*
- *A palette of acceptable colors is available from the Design Review Board. Color application should be used consistently throughout each home site for all the buildings and secondary structures.*
- *Colors for large field application shall be recessive in value, while accent colors should be used in limited areas.*



Porch Diagram

# 03 DESIGN DOCUMENTS

## 03.6 ARCHITECTURAL DESIGN GUIDELINES

### 03.6.5 Project and Building Signage

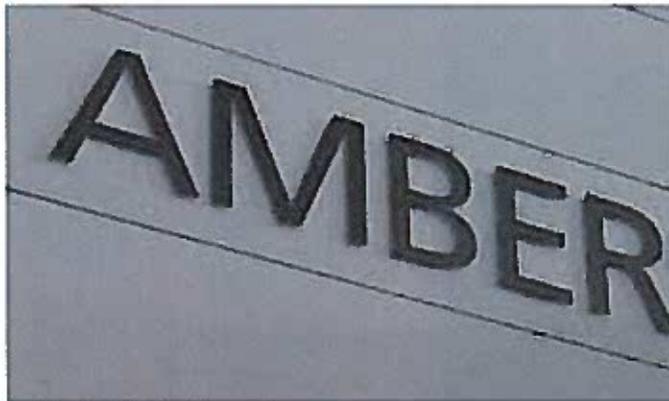
Project and building signage is defined to respond to the various identification needs throughout the Southwest Idaho Treatment Center Master Plan and to ensure that there is continuity of overall signage while allowing for specific individualization. All signage will be reviewed and approved by the Development Ownership Team and an Architectural Control Committee established by the development parameters. All development signage will be maintained by the various owner associations within the development areas. All individual tenant signage maintenance will be the responsibility of the tenant and or the specific building owner.

In addition, the following components are included as part of the Southwest Idaho Treatment Center Master Plan design guidelines:

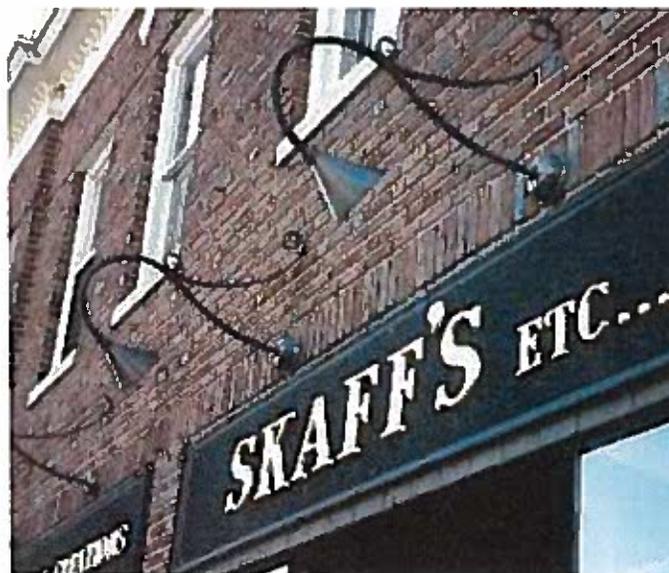
- Each tenant shall be required to submit signage plans to the building's managing entity for written approval. A design review sign application, accompanied by the written approval of the managing entity, shall be submitted to the City for any proposed signs for City staff review and approval. All signs shall be in compliance with the criteria stated herein as well as Nampa City Code. In cases of any conflict between Nampa City Code and the criteria specified herein, the more restrictive requirements shall apply.
- Building wall signs shall be placed within the approved sign areas as designated on individual project elevations. Signage may not exceed the standards in the Nampa City Zoning Ordinance signage standards.
- All building wall signs shall be constructed of a hi-density urethane material or aluminum 1/8" thick (minimum). The face of the wall signs shall have dimension by routing or extrusion, with a minimum of 1/4" variation. Individual letter signs shall meet the following:
  - Letters shall not be interconnected and a maximum of 12" in height.
  - Letters taller than 6" shall be a minimum of 1 1/2" in thickness.
  - Letters shall be stud mounted 1" off the face of the building.
  - Letters shall be flat or matte black or other approved earth tone colors. No glossy.
  - Paints, vinyl or Plexiglas is permitted.
- All elements shall be painted with an color palette sensitive to its context and land use district. If, in the opinion of City staff, a proposed color varies significantly from the colors depicted on the colored rendering submitted to the City for the monument signs or for the building, staff may choose to have the Design Review Committee review the color for appropriateness.
- There will be development specific monument signs identifying each development area in the Southwest Idaho Treatment Center Master Plan. Those locations will be identified on the final plot maps of each section.
- Buildings will be allowed a monument sign on each street frontage. The sign must conform to the Nampa City Zoning Ordinance signage standards in effect at the time a sign permit is submitted for.
  - Individual tenant identification is allowed on monument signs.
- Exterior illumination for the monument signs shall be recessed in the ground and shall be shielded with landscape materials.
- If exterior illumination is proposed for building wall signs, gooseneck light fixtures shall be used and shall be located as not to shine onto the roadway or onto any nearby property.
- Any free-hanging signboards proposed under the covered porch shall be a maximum of 8 square feet each. A maximum of one free-hanging signboard per tenant shall be permitted. If any such sign overhangs a walkway a minimum of 7-feet 6-inches shall be maintained between the bottom of the sign and the walkway.
- One neon sign per business shall be permitted and may not exceed in any dimension the designated signage area per Elevations.
- Any other signage such as temporary, grand opening, etc. shall comply with Nampa City Code.
- Any signage on doors or windows shall comply with Nampa City Code.



*Letters Mounted 1" Off Face of Wall*



*Flat Black Letters*



*Goosneck Light Fixtures*



APPLICATION FOR AMENDMENT OF COMPREHENSIVE PLAN
City of Nampa, Idaho

7/2/16 PR
KAVIA

This application must be filled out in detail and submitted to the office of the Planning Director for the City of Nampa, Idaho, accompanied by a nonrefundable fee of \$421.00 (for 1 acre or less), and \$842.00 (for more than 1 acre) for a map amendment; or \$213.00 for a text amendment.

Name of Applicant/Representative: Doug Russell / The Land Group Inc. Phone: 208.939.4041
Address: 462 E. Shore Dr. Suite 100 City: Eagle State: ID Zip Code: 83616
Applicant's interest in property: (circle one) Own Rent Other Planner / Design Consultant
Owner Name: Idaho Department of Health & Welfare (Paul Spannkebel) Phone: 208.334.5912
Address: 450 W. State St. City: Boise State: Idaho Zip Code: 83720

Address of subject property: 1660 11th Ave. N, Nampa, ID

Is a copy of one of the following attached? (circle one) Warranty Deed Proof Of Option Earnest Money Agreement.

Subject Property Information

(Please provide one form of the following REQUIRED DOCUMENTATION to complete the amendment):

[X] Original Legal description of property AND a legible WORD formatted document. (Must have for final recording) Old or illegible title documents will need to be retyped in a WORD formatted document

[ ] Subdivision Lot Block Book Page

Project Description

State (or attach a letter stating) the requested zoning, the land use change(s) and the reason for the proposed change(s) and the use(s) which will be involved: See attached narrative for further details

PARKS/PUBLIC TO COMMUNITY MIXED USE

If this application is for a change of plan text complete the following:

State (or attach a letter stating) the text changes requested, the page numbers in the plan, the reason for the proposed changes and why they would be in the interest of the public (attach the full text of the proposed amendment, as necessary):

N/A

Dated this 28th day of July, 2015

[Signature]
Signature of applicant

NOTICE TO APPLICANT

This application shall be referred to the Nampa Planning Commission for consideration at a public hearing. The Planning Commission will then make its recommendation to the City Council.

If the amendment is recommended for approval a second hearing shall be held before the City Council. If the amendment is recommended for denial you may appeal the decision to the City Council within 15 days from the date of such action by the Planning Commission. At least 15 days prior to each hearing, notice of time and place and a summary of the amendment(s) to be discussed shall be published in the Idaho Press-Tribune. In the case of map amendments notice shall also be posted on the premises not less than 1 week prior to the hearings and notices will be mailed to property owners or purchasers of record within 300 feet of the subject property. You will be given notice of the public hearings and should be present to answer any questions.

Any person may apply for a plan amendment at any time to correct errors in the original plan or to recognize substantial changes in the actual conditions of an area.

For Office Use Only:

File Number: CMP 2008 - 2015 Project Name: CMP PUBLIC/PARKS TO COMMUNITY

MIXED USE 615 ACRES STATE OF ID
+ NAMPA LANDS N. OF I84 - GOLF COURSE AREA



APPLICATION FOR AMENDMENT OF ZONING ORDINANCE OR MAP

City of Nampa, Idaho

Y1216 P2

KALH

This application must be filled out in detail and submitted to the office of the Planning Director for the City of Nampa, Idaho, accompanied by a nonrefundable fee of \$406.00 (for 1 acre or less), and \$811.00 (for more than 1 acre) for a map amendment; or \$213.00 for a text amendment.

Name of Applicant/Representative: Doug Russell / The Land Group Inc. Phone: 208.939.4041

Address: 462 E. Shore Dr. Suite 100 City: Eagle State: ID Zip Code: 83616

Applicant's interest in property: (circle one) Own Rent Other Planner / Design Consultant

Owner Name: Idaho Department of Health & Welfare (Paul Spannkebel) Phone: 208.334.5912

Address: 450 W. State St. City: Boise State: Idaho Zip Code: 83720

Address of subject property: 1660 11th Ave. N, Nampa, ID

Is a copy of one of the following attached? (circle one) Warranty Deed Proof Of Option Earnest Money Agreement.

Subject Property Information

(Please provide one form of the following REQUIRED DOCUMENTATION to complete the amendment):

[X] Original Legal description of property AND a legible WORD formatted document. (Must have for final recording) Old or illegible title documents will need to be retyped in a WORD formatted document

[ ] Subvision Lot Block Book Page

Project Description

State the zoning desired for the subject property: GBI

State (or attach a letter stating) the zoning amendment desired, text or map, and the reason for the change, together with any other information considered pertinent to the determination of the matter. In the case of a text amendment please attach the full text of the proposed amendment.

See attached narrative AG TO GB-1

Dated this 28th day of July, 2015

[Signature] Signature of applicant

NOTICE TO APPLICANT

This application will be referred to the Nampa Planning Commission for its consideration. The Planning Commission shall hold a public hearing on the application and will then make its recommendation to the City Council. The City Council will then hold a second public hearing. Notice of the public hearings must be published in the Idaho Press-Tribune 15 days prior to said hearings. In the case of map amendments notice shall also be posted on the premises not less than 1 week prior to the hearings and notices will be mailed to property owners or purchasers of record within 300 feet of the subject property. You will be given notice of the public hearings and should be present to answer any questions.

For Office Use Only:

File Number: REZ 2009 - 2015

Project Name: REZ AG TO GB-1 615.60 ACRES STATE OF ID. / CITY OF NAMPA N OF I-84 / GOLF COURSE AREA



# Application for Planned Unit Development

City of Nampa, Idaho

This application must be filled out in detail and submitted to the office of the Planning Director for the City of Nampa, Idaho, accompanied by the appropriate fee of \$790.00.

(This application may take place under the same fee concurrent with an application for rezoning or zoning with annexation provided the procedural requirements for each application are met. A separate fee shall be required when filing for approval of a preliminary and/or final development plan.)

Name of Applicant/Representative: Doug Russell/ The Land Group, Inc. Phone: 208.939.4041  
 Address: 462 E. Shore Dr. Suite 100 City: Eagle State: ID Zip Code: 83616  
 Applicant's interest in property: (circle one) Own Rent Other Planner/Design Consultant  
 Owner Name: Idaho Department of Health & Welfare (Paul Spannkebel) Phone: 208.334.5912  
 Address: 450 W. State St. City: Boise State: Idaho Zip Code: 83720  
 Survey, Engineer or Planner Name: The Land Group, Inc. Phone: 208.939.4041  
 Proposed Schedule for Development: Beginning Date 2017 to Ending Date 2045

Address of subject property: 1660 11th Ave. N, Nampa, ID

Is a copy of one of the following attached? (circle one) Warranty Deed Proof Of Option Earnest Money Agreement.

### Subject Property Information

(Please provide one form of the following REQUIRED DOCUMENTATION to complete the PUD request):

- Original Legal description of property AND a legible WORD formatted document.  
Old or illegible title documents will need to be retyped in a WORD formatted document
- OR Subdivision \_\_\_\_\_ Lot \_\_\_\_\_ Block \_\_\_\_\_ Book \_\_\_\_\_ Page \_\_\_\_\_
- A Concept Plan of the proposed development at a scale of at least 1"=100' showing all existing conditions and the proposed location and type of land uses.

### Project Description

State the zoning desired for the subject property: GBI

Existing Use of subject property: State facilities and golf course

State (or attach a letter stating) the reason the PUD would be in the Public Interest: See attached narrative for further details.

Dated this 30 day of June, 2016

Applicant Signature

### Notice to Applicant

This application will be referred to the Nampa Planning Commission for consideration at a public hearing. This first hearing shall be for the approval of the concept plan. If the concept plan is recommended to the City Council for approval, the City Council will hold a second public hearing for the purpose of endorsing the Commission's approval of the concept plan and amending the zoning map by overlay of the Planned Unit Development (PUD) district. The City Council may either approve, approve with conditions or deny the amendment. If conditions are attached, the City Council shall not amend the zoning map until the concept plan has been revised and approved by the Planning Commission. Notice of the public hearings must be published in the Idaho Press Tribune 15 days prior to said hearings. Notice shall be posted on the premises not less than 1 week prior to the hearings. Notice will also be mailed to property owners or purchasers of record within 300 feet of the subject property. You will be given notice of the public hearings and should be present to answer any questions.

Following concept plan approval you may proceed with preparation of the Preliminary Development Plan (PDP). Upon approval of the PDP by the Planning Commission, you may proceed with the preparation of the Final Development Plan (FDP). All plans must be prepared in accordance the Nampa Comprehensive Zoning Ordinance. The Planning Commission shall either approve or disapprove the FDP and submit their recommendation to the City Council. If the plan is either approved or approved with conditions the City Council shall authorize the Planning Director to issue a PUD permit in accordance with the approved plan and supplementary conditions attached thereto. If the Planning commission denies approval at any stage in the above process you may appeal the decision to the City Council with in 15 days from the date of such action.

### For Office Use Only:

File Number: PUD 002 - 2016 Project Name: Southwest Idaho Treatment Center



## Norm Holm

---

**From:** Doug Russell <doug@thelandgroupinc.com>  
**Sent:** Tuesday, June 14, 2016 10:51 AM  
**To:** Karla Nelson  
**Cc:** Norm Holm; Robert Hobbs  
**Subject:** SWITC- Comprehensive Plan and Rezone  
**Attachments:** 160614 SWITC Comp Plan Narrative Letter 111135.pdf; 160614 SWITC DRAFT Development Agreement Conditions 111135.pdf; MB 160614 SWITC 300scale 111135.pdf

All

As discussed via the phone over the past week, I am pleased to be sending you a revised narrative letter for the SWITC Comp plan and rezone applications. It is our intention that this letter and revised master plan exhibit get the SWITC public hearing process back on track once again. In addition to the attached exhibits, I will soon be providing you with a copy of our recent traffic analysis results and updated financial data based on current projections rather the projections presented back in 2014. One other piece of information that I am including is a copy of some development agreement conditions. These conditions were developed in concert with Clair Bowman in an effort to give him a level of confidence in the transportation plan going forward. We realize that actual language is subject to change, but these capture the overall vision in basic terminology.

We are excited to get this process moving forward. Please let me know if you have any questions or need any additional information. We look forward to being provide with a hearing date as soon as you know when that will be.

Thanks again for all your help.

Kind Regards, Doug

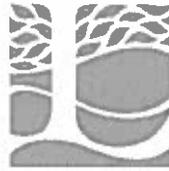
principal

---

**doug russell**

208.939.4041 | p  
462 e shore dr, ste 100  
eagle, idaho 83616  
[doug@thelandgroupinc.com](mailto:doug@thelandgroupinc.com)

THE LAND GROUP | [thelandgroupinc.com](http://thelandgroupinc.com)



THE LAND GROUP, INC.

June 14, 2016

Karla Nelson  
City of Nampa  
Planning and Zoning Department  
411 3<sup>rd</sup> Street South  
Nampa, Idaho 83651

**Re: Southwest Idaho Treatment Center- Comprehensive Plan Amendment and Rezone Applications**

Dear Karla

As you know, back in August of 2015 we requested a deferral of our public hearing to a date uncertain. Since that time, I have been working closely with Nampa's Senior Transportation Planner, Clair Bowman and our traffic consultant, John Ringert of Kittelson and Associates. The purpose of our collaboration has been to better understand the short and long term effects of traffic within the general vicinity of SWITC.

Utilizing growth projections from COMPASS, John and Clair developed and reviewed a model which allowed them to better understand the impacts of this project on existing and future traffic patterns, not only within the project limits but at various strategic intersections within a +/- 5mile radius. This understanding led to some key revisions within our master plan, with a specific purpose of accommodating SWITC traffic while at the same time improving traffic patterns within the planning area. This is achieved by providing another overpass crossing I-84 as well as another east west route connecting Garrity Blvd and Franklin Road. These key transportation elements will assist in the accommodation of anticipated growth as individual project phases come on line.

On behalf of the Idaho Department of Health and Welfare, I am excited to request that once again we move forward with public process as required for consideration of a comprehensive plan amendment, Planned Unit Development (PUD) and Rezone as allowed and outlined in the Nampa City Code. As discussed, we are providing the PUD application as required to accommodate residential development within the requested GB1 zone. We anticipate and encourage the inclusion of our revised overall master plan and design guidelines to ensure that the property is developed in a manner consistent with the owner's vision, Nampa's Comprehensive Plan and city ordinances.

The project site consists of seven existing parcels of land which total +/- 615 acres. The site is bounded by Interstate 84 to the south, the Union Pacific Railroad on the north and west, and commercial



THE LAND GROUP, INC.

July 24, 2015

Project No. 111135

**Exhibit "A"**

**SWITC PROPERTY DESCRIPTION**

A parcel of land located in Sections 11, 12, 13 and 14, Township 3 North, Range 2 West, Boise Meridian, Canyon County, Idaho, being more particularly described as follows:

Commencing at the Section Corner common to Sections 11, 12, 13 and 14 of said Township 3 North, Range 2 West;

Thence South  $00^{\circ}05'22''$  West, a distance of 2651.50 feet on the Section Line common to said Sections 13 and 14 to the One Quarter Section Corner common to said Sections 13 and 14;

Thence North  $89^{\circ}28'56''$  West, a distance of 2632.29 feet on the east-west mid-section line of said Section 14 to the Center One Quarter Section Corner of said Section 14;

Thence North  $00^{\circ}07'22''$  East, a distance of 305.19 feet on the north-south mid-section line of said Section 14 to the POINT OF BEGINNING;

Thence North  $00^{\circ}07'22''$  East, a distance of 989.85 feet on the north-south mid-section line of said Section 14 to a point on the West Bank of the Phyllis Canal;

Thence on the West Bank of the Phyllis Canal for the following courses and distances:

Thence North  $47^{\circ}39'38''$  West, a distance of 119.37 feet to a point of curve;

Thence 212.27 feet on the arc of a curve to the right, said curve having a radius of 254.00 feet, a central angle of  $47^{\circ}52'54''$ , a chord bearing of North  $23^{\circ}43'11''$  West, and a chord length of 206.14 feet;

Thence North  $00^{\circ}13'16''$  East, a distance of 191.89 feet;

Thence North  $02^{\circ}48'52''$  East, a distance of 133.40 feet to a point of curve;

Thence 351.38 feet on the arc of a curve to the right, said curve having a radius of 665.00 feet, a central angle of  $30^{\circ}16'29''$ , a chord bearing of N  $17^{\circ}57'07''$  East, and a chord length of 347.31 feet;

Thence North  $33^{\circ}05'21''$  East, a distance of 108.06 feet to a point on the north-south mid-section Section Line of said Section 14;

Thence leaving the West Bank of the Phyllis Canal, North  $00^{\circ}07'22''$  East, a distance of 248.91 feet on the north-south mid-section Section Line of said Section 14 to a point on a curve on the southerly right-of-way line of the Oregon Short Line Railroad property;

Thence on the southerly right-of-way line of the Oregon Short Line Railroad property for the following courses and distances:

Thence 470.22 feet on the arc of a curve to the right, said curve having a radius of 1818.34 feet, a central angle of  $14^{\circ}49'00''$ , a chord bearing of North  $33^{\circ}02'21''$  East, and a chord length of 468.91 feet;

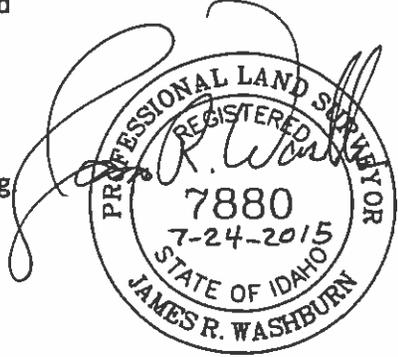
Thence North  $53^{\circ}45'23''$  East, a distance of 1848.32 feet to a point of curve;

Thence 1018.50 feet on the arc of a curve to the right, said curve having a radius of 2764.79 feet, a central angle of  $21^{\circ}06'24''$ , a chord bearing of North  $64^{\circ}18'35''$  East, and a chord length of 1012.75 feet to a point of compound curve;

Thence 813.57 feet on the arc of a curve to the right, said curve having a radius of 2764.79 feet, a central angle of 16°51'36", a chord bearing of North 83°17'35" East, and a chord length of 810.64 feet;  
 Thence South 88°16'37" East, a distance of 2936.18 feet;  
 Thence South 00°39'19" West, a distance of 558.56 feet;  
 Thence South 00°56'23" West, a distance of 601.67 feet;  
 Thence South 53°57'38" East, a distance of 302.44 feet;  
 Thence South 40°09'30" East, a distance of 582.79 feet;  
 Thence South 20°10'34" East, a distance of 344.13 feet;  
 Thence South 09°11'56" East, a distance of 639.93 feet;  
 Thence South 41°44'36" East, a distance of 677.11 feet to a point on the Northerly Right-of-Way line of Ridgecrest Drive;  
 Thence South 89°22'10" East, a distance of 164.71 feet on said Northerly Right-of-Way Line of Ridgecrest Drive;  
 Thence South 09°30'05" West, a distance of 56.29 feet to a point on the Southerly Right-of-Way Line of said Ridgecrest Drive;  
 Thence North 89°22'10" West, a distance of 108.48 feet on said Southerly Right-of-Way Line of Ridgecrest Drive to a point of curve;  
 Thence 205.68 feet on the arc of a curve to the right, said curve having a radius of 260.00 feet, a central angle of 45°19'31", a chord bearing of North 66°42'25" West, and a chord length of 200.36 feet on said Southerly Right-of-Way Line of Ridgecrest Drive;  
 Thence North 44°02'39" West, a distance of 14.42 feet on said Southerly Right-of-Way Line;  
 Thence South 10°12'56" West, a distance of 232.88 feet;  
 Thence South 31°00'42" West, a distance of 159.35 feet to a point of curve on the Northerly Right-of-Way Line of Interstate 84;  
 Thence on the Northerly Right-of-Way Line of Interstate 84 for the following courses and distance:

Thence 270.55 feet on the arc of a curve to the left, said curve having a radius of 711.62 feet, a central angle of 21°46'59", a chord bearing of South 76°30'54" West, and a chord length of 268.92 feet;  
 Thence South 65°37'25" West, a distance of 377.49 feet to a point of curve;  
 Thence 696.44 feet on the arc of a curve to the right, said curve having a radius of 2216.83 feet, a central angle of 18°00'00", a chord bearing of South 74°37'25" West, and a chord length of 693.58 feet;  
 Thence South 83°47'45" West, a distance of 728.23 feet;  
 Thence South 83°03'27" West, a distance of 200.25 feet;  
 Thence South 85°55'12" West, a distance of 1082.88 feet to a point of curve;  
 Thence 141.00 feet on the arc of a curve to the right, said curve having a radius of 22,808.30 feet, a central angle of 0°21'15", a chord bearing of South 86°05'50" West, and a chord length of 141.00 feet to a point of curve;  
 Thence 1199.06 feet on the arc of a curve to the right, said curve having a radius of 22,808.30 feet, a central angle of 03°00'45", a chord bearing of South 87°46'50" West, and a chord length of 1199.06 feet;  
 Thence South 89°17'12" West, a distance of 955.68 feet;  
 Thence South 89°20'27" West, a distance of 1173.68 feet;  
 Thence North 84°03'29" West, a distance of 533.49 feet;  
 Thence South 89°35'38" West, a distance of 133.54 feet to the point of beginning.

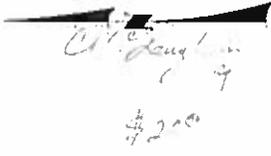
The above Parcel Contains 615.6 Acres more or less.  
 PREPARED BY: THE LAND GROUP, INC.  
 JAMES R. WASHBURN



A SURVEY FOR THE CITY OF NAMPA LOCATED IN SECTIONS 11, 12, 13 AND 14,  
TOWNSHIP 3 NORTH, RANGE 2 WEST, BOISE MERIDIAN, CANYON COUNTY, IDAHO.

RECORD OF SURVEY

21 10

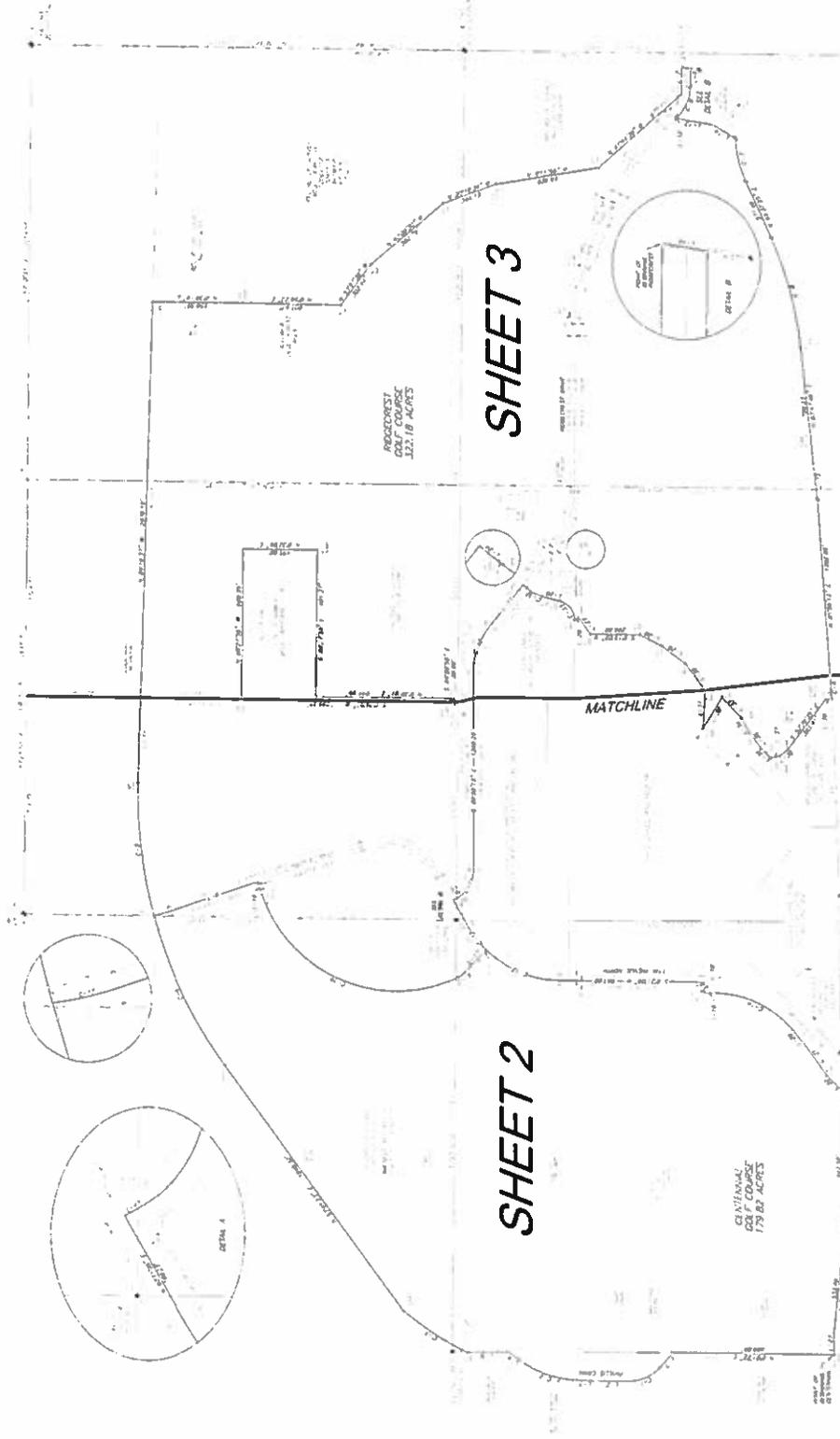


SURVEY INDEX NO. 324-11-0-0-0000  
 SURVEY INDEX NO. 324-12-0-0-0000  
 SURVEY INDEX NO. 324-13-0-0-0000  
 SURVEY INDEX NO. 324-11-0-0-0000

LEGEND

- FOUND 5/8" IRON PIN
- SET 5/8" IRON PIN WITH PLS 5082 PLASTIC CAP
- FOUND BRASS CAP MONUMENT
- FOUND AXLE OR RAILROAD SPIKE
- FOUND PK NAIL
- CALCULATED POINT NOTHING SET
- FOUND RED ROWN CONCRETE MONUMENT

SEE SHEET 4 FOR  
CURVE AND LINE DATA



RECORDED  
324-11-0-0-0000  
32.18 ACRES

ORIGINAL  
324-11-0-0-0000  
179.87 ACRES

SHEET 3

SHEET 2

MATCHLINE

DETAIL A

DETAIL B



*Working together to plan for the future*

July 5, 2016

Norm Holm, Director  
Nampa Planning & Zoning Department  
411 3rd Street South  
Nampa, ID 83651

**Re: Southwest Idaho Treatment Center Development Checklist**

Dear Mr. Holm:

On June 15, 2016, the Community Planning Association of Southwest Idaho (COMPASS) received the City of Nampa's request for a Comprehensive Plan Future Land Use Map amendment and rezone of the Southwest Idaho Treatment Center area. As a member service, COMPASS provides development review checklists to assist member agencies evaluate consistency of proposed development with the goals in the regional long-range transportation plan for Ada and Canyon Counties: *Communities in Motion 2040* (CIM 2040). CIM 2040 was developed collaboratively with land use and transportation agencies throughout the Treasure Valley and the demographic forecasts for households and jobs in CIM 2040 are based upon the approved comprehensive plans of the cities and counties.

Attached is the development review checklist for the Southwest Idaho Treatment Center property. Important highlights the City of Nampa may consider during its review process include:

- The proposed development is anticipated to create about 3,400 more jobs than forecasted for the area. This could place a tremendous demand on the transportation system, whereas system improvements to accommodate that level of employment have not been planned for as they were not anticipated during the development of the region's long range transportation plan. If the proposal passes, COMPASS and Nampa will need to reconcile the forecasted demographics for the area and include capital improvements needed from the developer's Traffic Impact Study in the next long range transportation plan.
- The application states the development will include a "Transit Center" at an undetermined location on the property. In 2015, COMPASS developed a public transportation network as part of the long range plan. This network is in the process of being further refined in the long range plan update (*Communities in Motion 2040 2.0*). While high frequency services were identified for Idaho Center Boulevard, no services or amenities were planned in the Southwest Idaho Treatment Center area. It is important to note this is primarily due to the planned density and land uses previously assumed at these locations.

- Ongoing evaluation of a high capacity transit corridor parallel I-84 is ongoing. While specific alignments and transit modes have not been selected, it is important to note one potential option for high capacity transit runs along the northern boundary of the subject property. Currently, a station location is proposed at the College of Western Idaho Campus/Idaho Center Blvd area. It will be critical to maintain compatible land uses and access to this potential location. If the applicant desires another transit center location, please coordinate closely with Valley Regional Transit and COMPASS in the refinement of the future public transportation network update, which will be solidified in the next six months.
- The Complete Streets Level of Service (CSLOS) scorecard (included in the checklist) is an estimation of the anticipated bicycle, pedestrian and transit traffic level of comfort on an arterial roadway. The scorecard was completed using the assumption that the main east-west road connecting Franklin Road to Karcher Road would be a 5 lane facility. COMPASS can provide a similar analysis with alternative street designs or for other roadways. If you have questions on this analysis please call.
- To meet the policies of the 2011 City of Nampa Bicycle and Pedestrian Master Plan consider preserving easements along the Union Pacific Railroad line of the project as this rail line is being considered for future adjacent Rails with trails efforts. Nampa participates in the regional Rails with Trails consortium led by COMPASS. Preservation of these easements is a fantastic way to provide these regional connections. Additionally, the City of Nampa Bicycle and Pedestrian Master Plan highlights future shared bike lanes along 11th Avenue.

The checklist provided is not intended to be comprehensive, but provides a starting point for bridging local and regional planning. COMPASS looks forward to working with the City of Nampa, Idaho Transportation Department, and Valley Regional Transit regarding the conceptual master plan for the Southwest Idaho Treatment Center.

Please contact Carl Miller at [cmiller@compassidaho.org](mailto:cmiller@compassidaho.org) or (208) 475-2239 if you have any questions regarding the attached information or would like additional information.

Sincerely,



Sabrina C. Minshall  
Director of Planning

Attachment: Southwest Idaho Treatment Center Development Checklist

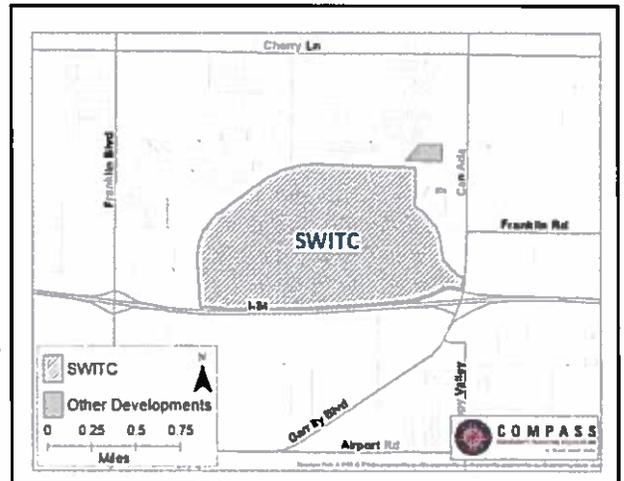
pc: Karla Nelson  
Jeff Barnes  
Len Grady  
Clair Bowman  
Amy Schroeder

T:\FY16\600 Projects\620 Demographics and Growth Monitoring\Development Monitoring\Development Tracking\Developments\Nampa SWITC Comp Plan Amend Req

# Communities in Motion 2040 Development Checklist

The Community Planning Association of Southwest Idaho (COMPASS) is the metropolitan planning organization (MPO) for Ada and Canyon Counties. COMPASS has developed this checklist as a tool for local governments to evaluate whether land developments are consistent with the goals of *Communities in Motion 2040* (CIM 2040), the regional long-range transportation plan for Ada and Canyon Counties. CIM 2040 was developed through a collaborative approach with COMPASS member agencies and adopted by the COMPASS Board on **July 21, 2014**.

This checklist is not intended to be prescriptive, but rather a guidance document based on CIM 2040 goals, objectives, and performance measures. A checklist user guide is available [here](#); and more information about the CIM 2040 goals can be found [here](#); and information on the CIM 2040 Vision can be found [here](#).



[Click here to view enlarged map](#)

**Name of Development:** Southwest Idaho Treatment Center - CMA/ZMA of 615.6 acres

**Summary:** Bounded by I-84 to the south, the Union Pacific Railroad to the north and west and Garry Blvd/Idaho Center Blvd/Can Ada Rd to the east, the proposed development would create 458 new residential units and approximately 5,040 jobs. There are two other active developments in the TAZs directly adjacent to this proposal, these developments are anticipated to add a total of 178 residential units.  
The proposal supports 17 CIM 2040 checklist items and does not support 6 CIM 2040 checklist items.

## Land Use

In which of the [CIM 2040 Vision Areas](#) is the proposed development? (**Goal 2.1**)?

- Downtown       Employment Center       Existing Neighborhood       Foothills  
 Future Neighborhood       Mixed Use       Prime Farmland       Rural  
 Small Town       Transit Oriented Development

Yes  No  N/A The proposal is within a CIM 2040 Major Activity Center. (**Goal 2.3**)

## Neighborhood (Transportation Analysis Zone) Demographics

TAZ: 2401 & 2402

Existing		Existing TAZ + Proposal		2040 Forecast	
Households	Jobs	Households	Jobs	Households	Jobs
0	186	458	5,040	465	1,629

Yes  No  N/A The number of jobs and/or households in this development is consistent with jobs/households in the CIM 2040 Vision in this neighborhood. (**Goal 2.1**)

## Area (Adjacent Transportation Analysis Zone) Demographics

TAZs: 2356, 2357, 2382, 2384, 2388, 2423, 2425, 2440, 2441, 2442

Existing		Existing TAZs + Net Proposed		2040 Forecast	
Households	Jobs	Households	Jobs	Households	Jobs
971	4,568	1,607	9,608	1,698	8,961

Yes  No  N/A The number of jobs and/or households in this development is consistent with jobs/households in the CIM 2040 Vision in this area. (**Goal 2.1**)



More information on COMPASS and *Communities in Motion 2040* can be found at:  
[www.compassidaho.org](http://www.compassidaho.org)  
 Email: [info@compassidaho.org](mailto:info@compassidaho.org)  
 Telephone: (208) 475-2239



**COMPASS**  
 COMMUNITY PLANNING ASSOCIATION  
 of Southwest Idaho

# Communities in Motion 2040 Development Checklist

## Transportation

- Attached  N/A An Area of Influence Travel Demand Model Run is attached.  
 Yes  No  N/A There are relevant projects in the current Regional [Transportation Improvement Projects](#) (TIP) within one mile of the development.

### Comments:

- Yes  No  N/A The proposal uses appropriate access management techniques as described in the [COMPASS Access Management Toolkit](#).

**Comments:** The proposal does not appear to conflict with the Access Management Toolkit.

- Yes  No  N/A This proposal supports Valley Regional Transit's [valleyconnect](#) plan. See [Valley Regional Transit Amenities Development Guidelines](#) for additional detail.

**Comments:** Future routes are proposed near the development. Applicant should contact VRT regarding transit center location and design.

The **Complete Streets Level of Service (LOS) scoring** based on the proposed development will be provided on an separate worksheet (**Goals 1.1, 1.2, 1.3, 1.4, 2.4**):

- Attached  N/A Complete Streets LOS scorecard is attached.  
 Yes  No  N/A The proposal maintains or improves current automobile LOS.  
 Yes  No  N/A The proposal maintains or improves current bicycle LOS.  
 Yes  No  N/A The proposal maintains or improves current pedestrian LOS.  
 Yes  No  N/A The proposal maintains or improves current transit LOS.  
CSLOS attachment indicates comfort level based on bicycle and pedestrian improvements on Karcher and Franklin  
 Yes  No  N/A The proposal is in an area with a [Walkscore](#) over 50.

## Housing

- Yes  No  N/A The proposal adds [compact housing](#) over seven residential units per acre. (**Goal 2.3**)  
 Yes  No  N/A The proposal is a mixed-use development or in a mixed-use area. (**Goal 3.1**)  
 Yes  No  N/A The proposal is in an area with lower transportation costs than the [regional average](#) of 26% of the median household income. (**Goal 3.1**)  
 Yes  No  N/A The proposal improves the jobs-housing balance by providing housing in employment-rich areas. (**Goal 3.1**)

## Community Infrastructure

- Yes  No  N/A The proposal is infill development. (**Goals 4.1, 4.2**)  
 Yes  No  N/A The proposal is within or adjacent to city limits. (**Goals 4.1, 4.2**)  
 Yes  No  N/A The proposal is within a city area of impact. (**Goals 4.1, 4.2**)

## Health

- Yes  No  N/A The proposal is within 1/4 mile of a transit stop. (**Goal 5.1**)  
 Yes  No  N/A The proposal is within 1/4 mile of a public school. (**Goal 5.1**)  
 Yes  No  N/A The proposal is within 1/4 mile of a grocery store. (**Goal 5.1**)  
 Yes  No  N/A The proposal is within 1 mile of a park and ride location. (**Goal 5.1**)

## Economic Development

- Yes  No  N/A The proposal improves the jobs-housing balance by providing employment in housing-rich areas. (**Goal 3.1**)  
 Yes  No  N/A The proposal provides grocery stores or other retail options for neighborhoods within 1/2 mile. (**Goal 6.1**)

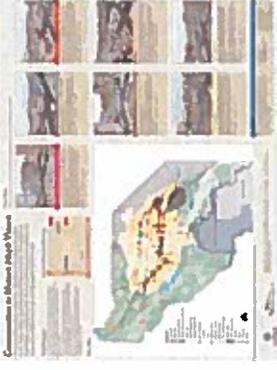
## Open Space

- Yes  No  N/A The proposal is within a 1/4 mile of a public park. (**Goal 7.1**)  
 Yes  No  N/A The proposal provides at least 1 acre of parks for every 35 housing units. (**Goal 7.1**) \*Proposals with multi-family housing are not subject to park criteria.

## Farmland

- Yes  No  N/A The proposal is outside "Prime Farmland" in the CIM 2040 Vision. (**Goals 4.1, 8.2**)  
 Yes  No  N/A The proposal is outside prime farmland. (**Goal 8.2**)

## Communities in Motion 2040 Complete Streets Scorecard



The purpose of this checklist is to provide a tool for local governments to evaluate whether land developments are in accordance with the goals of *Communities in Motion 2040* (CIM 2040). Complete Streets Level of Service (LOS) refers to the multimodal (automobile, bicycle, pedestrian, and transit) experience and grades a roadway (A-F) for each mode. COMPASS conducts Complete Streets Level of Service (CSLOS) analysis for developments on arterial roads.

Street	Mode	5-lane roadway	5-lane with sidewalk	5-lane with sidewalk with bicycle lane
		Link LOS	Link LOS	Link LOS
Franklin to Karcher Idaho Center Boulevard to Franklin Boulevard	Transit	F	F	F
	Bike	D	D	B
	Ped	E	C	C
Highway Capacity Manual 2010 Methodologies				

More information on  
COMPASS and  
*Communities in Motion*  
2040 can be found at:



[www.compassidaho.org](http://www.compassidaho.org)



**COMPASS**  
COMMUNITY PLANNING ASSOCIATION  
of Southwest Idaho

# Communities in Motion 2040 Checklist User Guide

**C**ommunity Planning Association of Southwest Idaho (COMPASS) is a forum for regional collaboration that helps maintain a healthy and economically vibrant region, offering people choices in how and where they live, work, play, and travel. COMPASS serves as the metropolitan planning organization (MPO) for Ada and Canyon Counties, Idaho. More information about COMPASS can be found [here](#).

**C**IM 2040 describes the current transportation system, outlines what is needed to accommodate future growth, explores how to fund future transportation needs, discusses how to maintain a safe and secure transportation system, and examines the environmental issues that have the potential to impact, or be impacted by, transportation investments. More information about the CIM 2040 can be found [here](#).

**H**ighlighted text indicates that there is a hyperlink to a document on the internet that further explains the background, topic, or methodology used to develop the information.

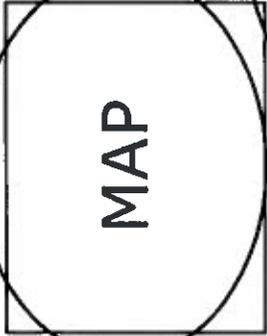
**Y**es and no answers to each of the 8 CIM 2040 elements have been developed to indicate how well a development aligns with the goals and vision of CIM. More information about the eight CIM 2040 goals, objectives, and tasks can be found [here](#).

**Q**R Codes (or Quick Response Codes) can be scanned with a personal electronic device, such as a cell phone or tablet, to open a web page. The QR code on the CIM 2040 land development checklist will take the user to the COMPASS home page ([www.compassidaho.org](http://www.compassidaho.org)).

## Communities in Motion 2040 Development Checklist

The Community Planning Association of Southwest Idaho (COMPASS) is a metropolitan planning organization (MPO) for Ada and Canyon Counties. COMPASS has developed this checklist as a tool for local governments to evaluate whether land development is consistent with the goals of *Communities in Motion 2040* (CIM 2040) and long-range transportation planning for Ada and Canyon Counties. CIM 2040 was developed through a collaborative approach with COMPASS member agencies and adopted by the COMPASS Board on July 21, 2014.

This checklist is not intended to be prescriptive, but rather a guidance document based on CIM 2040 goals, objectives, and outcomes. A checklist user guide is available [here](#), and more information about the CIM 2040 Vision can be found [here](#) and information on the CIM 2040 Vision can be found [here](#).



**L**ocation map highlights the location of the proposed development and the nearby 2040 functionally classified roadways. Functionally classified classification groups roads into classes according to the character of service they are intended to provide. More information on functional classification can be found [here](#).

**C**IM 2040 Vision Areas highlight the general vision for how the region will grow. Each typology describes different uses and densities as well as approaches to promoting housing, economic development, open space, farmland, health, and community infrastructure. More information about the CIM 2040 Vision is found [here](#).

**M**ajor Activity Centers (MACs) are important trip generators and are logical destinations for public transit service. MACs tend to increase productivity and support economic development, reduce land consumption and sprawl, and provide options for those that are carless. More information about Major Activity Centers can be found [here](#).

**T**raffic Analysis Zones (or TAZs) are designated areas used for travel demand modeling. CIM 2040 was developed using TAZs at the core unit of geography. "Neighborhood demographics" refer to exact TAZs where the development will occur, while "area demographics" refer to the neighborhood TAZs plus TAZs adjacent to the development area to provide an overview of the impact on the broader area. More information about the regional travel demand model can be found [here](#).

**C**OMPASS staff welcome the opportunity to discuss *Communities in Motion*, the checklist, or specific development proposals with stakeholders. Please feel free to contact us at to schedule an appointment using the phone number/email address to the left. More information on the COMPASS member service can be found [here](#).

Name of Developments \_\_\_\_\_

Summary: \_\_\_\_\_

### Land Use

In which of the following land uses is the proposed development? (Goal 2.1.1)

- Downtown
- Future Neighborhood
- Small Town
- Suburban Center
- Transit Oriented Development
- Existing Neighborhood
- Prime Farmland
- Food/Bills
- Rural

Yes  No  N/A The proposal is within a CIM 2040 Major Activity Center. (Goal 2.1)

### Neighborhood (Transportation Analysis Zone) Demographics

Neighborhood	Existing + Proposed		2040 Forecast	
	Jobs	Households	Jobs	Households
1				
2				
3				
4				
5				
6				
7				
8				

Yes  No  N/A The number of jobs and/or households in this development is consistent with jobs/households in the CIM 2040 Vision in this neighborhood. (Goal 2.1)

### Area (Adjacent Transportation Analysis Zone) Demographics

Area	Existing + Proposed		2040 Forecast	
	Jobs	Households	Jobs	Households
1				
2				
3				
4				
5				
6				
7				
8				

Yes  No  N/A The number of jobs and/or households in this development is consistent with jobs/households in the CIM 2040 Vision in this area. (Goal 2.1)



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 Telephone: (208) 475-2239



# Communities in Motion 2040 Checklist User Guide

**A**rea of Influence travel demand model runs summarize the impact of the proposed development on the transportation network. More information on the travel demand model can be found [here](#).

**C**omplete Streets Level of Service (CSLOS) refers to the multimodal (automobile, bicycle, pedestrian, and transit) experience and grades a roadway (A-F) according to those modes. COMPASS conducts CSLOS analysis for developments on arterial roads when a traffic impact study is provided. A separate CSLOS worksheet will be provided as applicable. More information on the Complete Streets can be found [here](#).

**A**"location affordability portal" has been developed to determine housing and transportation costs. On average, in Ada and Canyon Counties, 26% of household income is spent on transportation. Developing in areas with existing employment and services will enable households to save on transportation costs." Developing in areas with employment and services will enable households to save. More information on the location affordability portal is available [here](#).

**I**nfill development is using land within already developed areas and meets the following criteria:

- At least 1 job/per acre within 1 mile of the development
- Within city limits (or enclave)
- Within 1/4 mile of at least one of the following: public school, public park, transit stop, or retail center.

More information on infill can be found [here](#).

**P**rime Farmland areas were designated in the CIM 2040 Vision as areas to preserve to enable agricultural production. While not all farmland will be able to be preserved over the next three decades, areas with prime soils and without urban infrastructure are key candidates for preservation. More information about the CIM 2040

## Communities in Motion 2040 Development Checklist

Transportation	Area of Influence Travel Demand Model	CSLOS	Complete Streets	Location Affordability	Housing	Community Infrastructure	Health	Economic Development	Open Space	Farmland	
Attached	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	
Yes	No	No	No	No	No	No	No	No	No	No	
Comments:	Area of Influence Travel Demand Model (AITDM) is used to estimate the impact of proposed development on the transportation network. More information on the travel demand model can be found <a href="#">here</a> .										
Comments:	Complete Streets Level of Service (CSLOS) refers to the multimodal (automobile, bicycle, pedestrian, and transit) experience and grades a roadway (A-F) according to those modes. COMPASS conducts CSLOS analysis for developments on arterial roads when a traffic impact study is provided. A separate CSLOS worksheet will be provided as applicable. More information on the Complete Streets can be found <a href="#">here</a> .										
Comments:	"location affordability portal" has been developed to determine housing and transportation costs. On average, in Ada and Canyon Counties, 26% of household income is spent on transportation. Developing in areas with existing employment and services will enable households to save on transportation costs." Developing in areas with employment and services will enable households to save. More information on the location affordability portal is available <a href="#">here</a> .										
Comments:	Infill development is using land within already developed areas and meets the following criteria: <ul style="list-style-type: none"> <li>• At least 1 job/per acre within 1 mile of the development</li> <li>• Within city limits (or enclave)</li> <li>• Within 1/4 mile of at least one of the following: public school, public park, transit stop, or retail center.</li> </ul> More information on infill can be found <a href="#">here</a> .										
Comments:	Prime Farmland areas were designated in the CIM 2040 Vision as areas to preserve to enable agricultural production. While not all farmland will be able to be preserved over the next three decades, areas with prime soils and without urban infrastructure are key candidates for preservation. More information about the CIM 2040										

**T**he Regional Transportation Improvement Program (TIP) is a short-range (3-5 year) capital improvement program (budget) of transportation projects consistent with federal regulations and area policies and strategies. The TIP lists all projects for which federal funds are anticipated, along with non-federally funded projects that are regionally significant. The TIP represents the transportation improvement priorities of the region and is required by federal law. Relevant projects include those that improve capacity or function of road; studies are not included. More information on the TIP can be found [here](#).

**V**alley Regional Transit's Bus Stop Location and Transit Amenities Development Guidelines help in designing and placing transit facilities/amenities. These guidelines are to be considered in within the context of the overall location and project. More information about the guidelines can be found [here](#).

**W**alkscore is a walkability index that assigns a numerical walkability on a scale from 0 - 100 based on walking routes to destinations such as grocery stores, schools, parks, restaurants, and retail. Scores of 50 or higher are considered at least "Somewhat Walkable" while scores less than 50 are "Car-Dependent." More information on the Walkscore can be found [here](#).

**C**ompact housing, in certain locations, can be an effective way to reduce housing burdens; promote walking, biking, and transit use; reduce infrastructure costs; and preserve valuable prime farmland. Examples of successful compact housing projects in the region can be found [here](#).

**W**hile individuals differ, most studies indicate that people are willing to walk up to 1/4 mile for transit, parks, schools, and grocery stores. After that walk distance, most travel is made by motorized vehicles. A map highlighting walkability to key landmarks is found [here](#).

**P**rime farmland is defined as having irrigable soils and water rights. More information on the farmland preservation can be found [here](#).

# Communities in Motion 2040 Vision

The *Communities in Motion 2040 Vision* illustrates a preferred growth scenario for the Treasure Valley, specifically Ada and Canyon Counties. Defined by local stakeholders, including the public, the *Vision* will help guide development of the *Communities in Motion 2040 regional long-range transportation plan*.

## Vision Statement

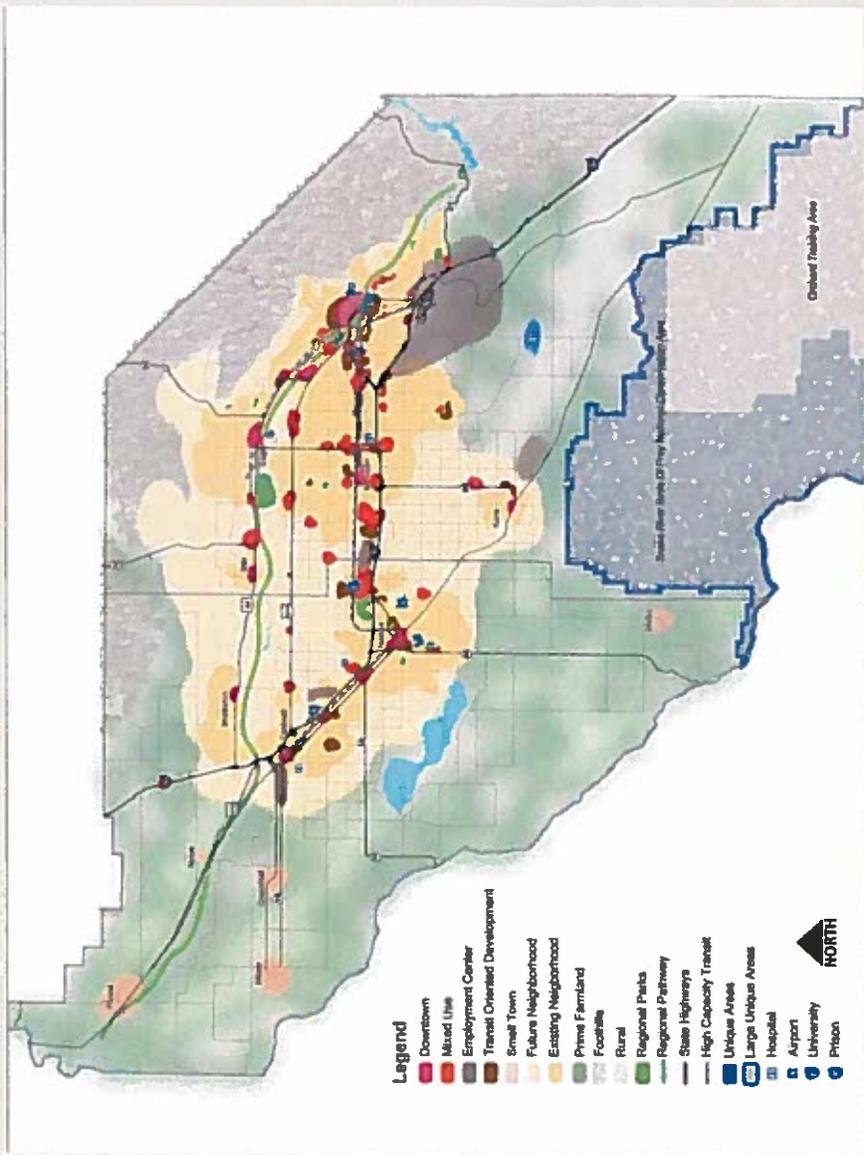
The *Communities in Motion 2040 Vision* provides a preferred growth scenario and a long-term vision for the region's future. It is a vision that is based on a variety of housing types, varied by infrastructure, equity, access, and outside of prime farmland or environmental constraints.

This scenario supports local comprehensive plan goals and objectives, and includes a goal to support high-capacity transit for State Street (Highway 4) and a transit corridor to Interstate 84, as well as multimodal infrastructure and services throughout the region.

Key goals include: supporting transit, increasing employment, increasing transportation options, improving job-housing balance, better access to parks, and maintaining environmental resources.

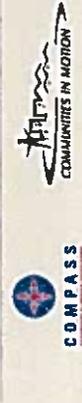
## Vision Map

## Land Use Density and Diversity



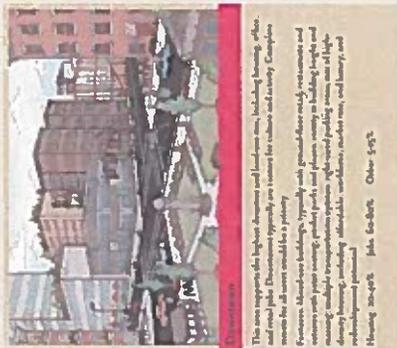
Please note: The *Vision Map* reflects the preferred growth scenario approved by the COMPASS Board. It is not a plan and has no regulatory authority.

The *Communities in Motion 2040 Vision* (COMPASS) is an initiative of local governments working together to plan for the future of the region. COMPASS members made it very clear that they want to see a high quality of life for every resident while making decisions about transportation and setting priorities for spending local transportation dollars over the next 20 years.

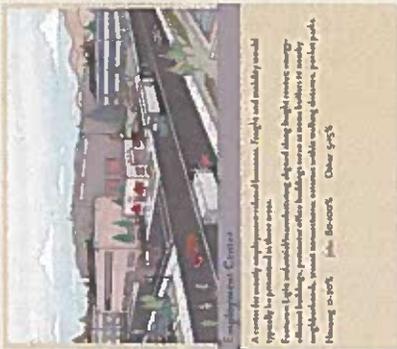


## Vision Areas

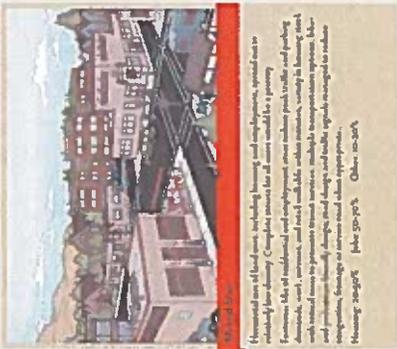
Below are possible interpretations of how the land use types included in *Communities in Motion 2040* would look.



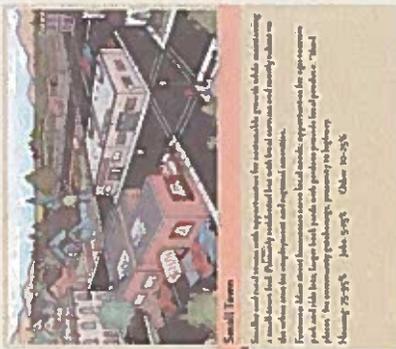
**Downtown**  
This area supports high-density urban uses, including housing, offices, and retail. Downtown typically uses transit to reduce land use. Complete streets for all users would be a priority.  
**Future:** Medium-rise buildings, typically with ground-floor retail, restaurants, and offices with upper floors for parking, transit, and transit-oriented development. High-density housing, including affordable, workforce, market rate, and luxury, and redevelopment projects.  
Housing: 30-40% Jobs: 60-80% Other: 5-5%



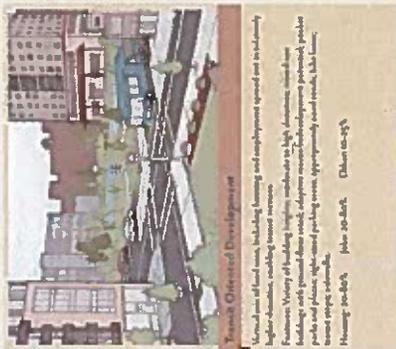
**Transit Oriented Development**  
Mixed-use of land uses, including housing and employment, spread out to reduce higher-density, walking transit access.  
**Future:** Variety of building heights, including high-density, medium-density, and ground-floor retail, adjacent to transit-oriented development projects. Parks and places, including parking areas, transit-oriented transit, bike lanes, transit stops, and transit.  
Housing: 30-40% Jobs: 60-80% Other: 10-15%



**Future Neighborhood**  
Neighborhoods prepared to be built, with sufficient opportunities for planned housing and transit-oriented development. Transit-oriented development is a key focus.  
**Future:** Range of housing types, including high-density, medium-density, and ground-floor retail, adjacent to transit-oriented development projects. Parks and places, including parking areas, transit-oriented transit, bike lanes, transit stops, and transit.  
Housing: 30-40% Jobs: 60-80% Other: 10-15%



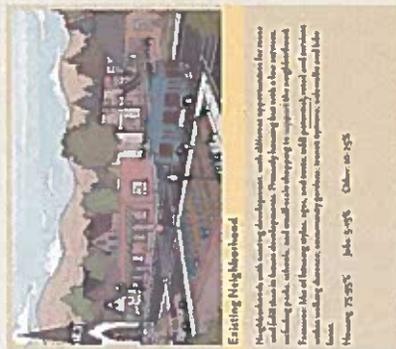
**Small Town**  
Small and rural areas with opportunities for sustainable growth while maintaining a small town feel. Planning and zoning for local services and transit-oriented development are a focus.  
**Future:** More transit-oriented development, including transit-oriented development projects and places, including parking areas, transit-oriented transit, bike lanes, transit stops, and transit.  
Housing: 30-40% Jobs: 5-5% Other: 10-15%



**Existing Neighborhood**  
Neighborhoods with existing development, with sufficient opportunities for transit-oriented development. Transit-oriented development is a key focus.  
**Future:** Range of housing types, including high-density, medium-density, and ground-floor retail, adjacent to transit-oriented development projects. Parks and places, including parking areas, transit-oriented transit, bike lanes, transit stops, and transit.  
Housing: 30-40% Jobs: 5-5% Other: 10-15%



**Future Neighborhood**  
Neighborhoods prepared to be built, with sufficient opportunities for planned housing and transit-oriented development. Transit-oriented development is a key focus.  
**Future:** Range of housing types, including high-density, medium-density, and ground-floor retail, adjacent to transit-oriented development projects. Parks and places, including parking areas, transit-oriented transit, bike lanes, transit stops, and transit.  
Housing: 30-40% Jobs: 5-5% Other: 10-15%



**Future Neighborhood**  
Neighborhoods prepared to be built, with sufficient opportunities for planned housing and transit-oriented development. Transit-oriented development is a key focus.  
**Future:** Range of housing types, including high-density, medium-density, and ground-floor retail, adjacent to transit-oriented development projects. Parks and places, including parking areas, transit-oriented transit, bike lanes, transit stops, and transit.  
Housing: 30-40% Jobs: 5-5% Other: 10-15%

- Boise State University
- College of Idaho
- Northwest Nazarene University
- College of Western Idaho
- St. Alphonsus Medical Center (Boise, Eagle, and Nampa)
- St. Luke's Medical Center (Boise and Meridian)
- Boise International Airport
- Nampa Airport, Caldwell Airport

Avoid for special considerations, including regional higher education centers and regional medical centers that have regional impact that they don't fit other center typologies. These areas will differ by type of use, including, and layout. They include airports (A), hospitals (H), prison (P), and university (U). In the Treasure Valley, page 18.

## Communities in Motion is the regional long-range transportation plan for Ada and Canyon Counties.

It offers a vision that addresses:

- How land use affects transportation
- How investment in transportation influences growth
- What the transportation system is supposed to achieve
- How transportation projects are selected
- How transportation projects serve regional needs

It is based on:

- **Centerlines:** Providing options for safe access and expanded mobility choices in a cost-effective manner in the region.
- **Coordination:** Achieving better inter-jurisdictional coordination of transportation and land use planning.
- **Environment:** Minimizing transportation impacts to people, cultural resources, and the environment.
- **Information:** Coordinating data gathering and disseminating better information.

The complete *Communities in Motion 2040* plan is available online at [www.compassidaho.org](http://www.compassidaho.org)

## Vision Benefits

**Economic Development:** A 6% increase in composite population near downtowns and other activity centers. Growth in these areas is typically more sustainable than other locations due to the proximity of features.

**Housing:** Growth in areas with transportation and other infrastructure improve overall affordability by locating housing near transit routes, employment centers, and basic services.

**Land Use:** Better job-housing balance reduces traffic, improves air quality, and increases discretionary time.

**Transportation:** Strong transportation infrastructure and services promote economic development and quality of life.

**Open Space:** Access to parks and open spaces enables citizens to enjoy the natural beauty of the region.

**Health:** More transportation options and development near services enables physical activity and improves air quality.

**Farmstead:** Almost 50% of farmland can be preserved by developing infill sites and other non-farm areas. This will increase agricultural economic value in the area and preserve food security.

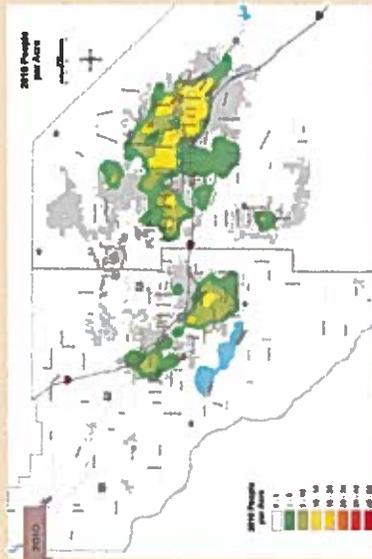
**Community Infrastructure:** Development in or nearby areas served by infrastructure reduces infrastructure costs and can save municipalities millions of maintenance and operations costs.

## Vision Demographics

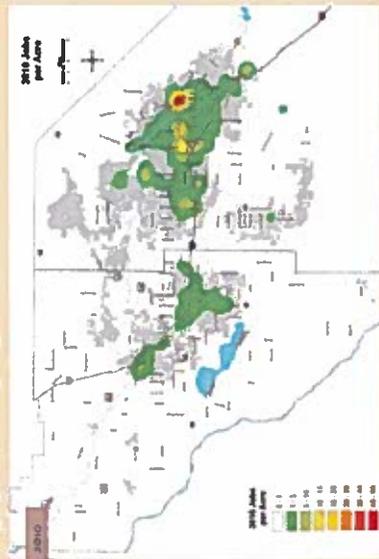
City/Area of Interest	2010		2040		Jobs	Population	Households	Jobs	Population	Households	Jobs
	Population	Households	Population	Households							
Burns	29,244	9,654	41,618	14,146	131,500	439,144	151,345	195,293	695,293	245,345	195,293
Engle	7,933	2,697	10,216	3,415	15,498	50,803	18,076	23,076	75,876	26,076	33,076
Grandon City	2,949	989	3,934	1,311	5,894	19,294	6,747	8,794	29,294	10,000	13,000
Gene	15,099	4,981	18,666	6,222	28,094	91,794	31,294	40,294	132,794	45,294	58,294
Madison	61,466	20,777	81,466	27,488	121,466	398,466	132,466	171,466	568,466	198,466	257,466
Star	6,471	2,157	8,628	2,876	12,876	42,876	14,876	19,876	64,876	21,876	28,876
Ada County (outside area of impact)	12,426	5,995	16,426	5,666	24,426	79,426	27,426	36,426	118,426	40,426	53,426
Ada County Total	82,355	28,445	112,355	37,445	166,355	544,355	188,445	251,355	834,355	288,445	384,355
Caldwell	59,612	19,440	81,440	27,440	121,440	398,440	132,440	171,440	568,440	198,440	257,440
Camas	1,248	410	1,658	547	2,458	8,058	2,747	3,658	12,058	4,047	5,348
Malheur	845	279	1,124	380	1,524	5,024	1,673	2,224	7,324	2,473	3,274
McMinn	2,148	714	2,862	954	4,262	14,162	4,711	6,262	20,662	6,911	9,162
Nezperce	961	319	1,280	429	1,880	6,180	2,069	2,780	9,180	3,069	4,080
Power	1,348	450	1,848	616	2,548	8,448	2,817	3,748	12,448	4,117	5,448
Wilder	1,599	531	2,199	731	3,099	10,199	3,378	4,599	15,199	5,078	6,799
Canyon County (outside area of impact)	22,534	7,534	30,534	10,234	45,534	148,534	50,534	65,534	213,534	71,534	93,534
Canyon County Total	68,913	23,068	91,913	30,268	136,913	445,913	151,068	198,913	641,913	213,068	281,913
Total Region	231,289	78,049	319,289	104,419	486,289	1,619,289	548,049	721,289	2,368,289	791,049	1,048,289

Note: Total employment data for non-employing sector is approx.

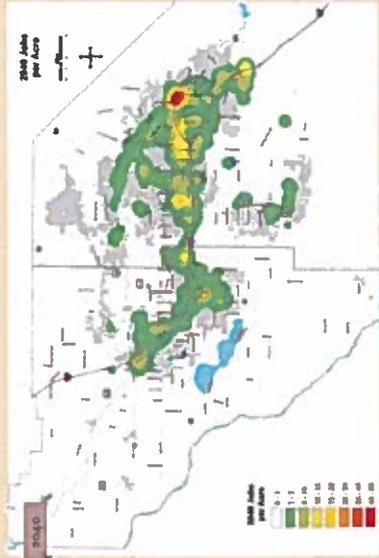
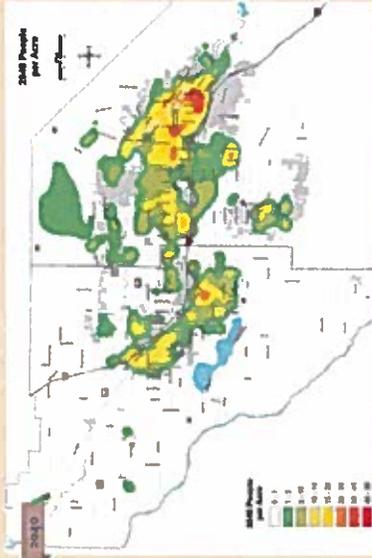
## Population Density Maps



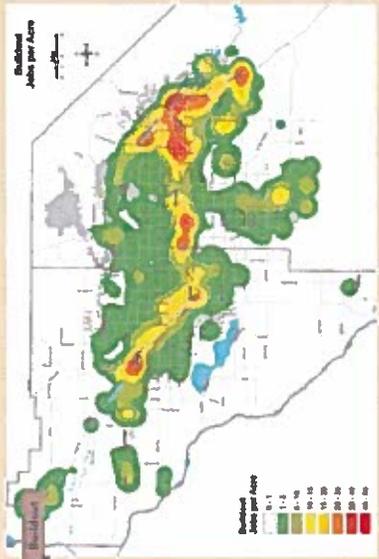
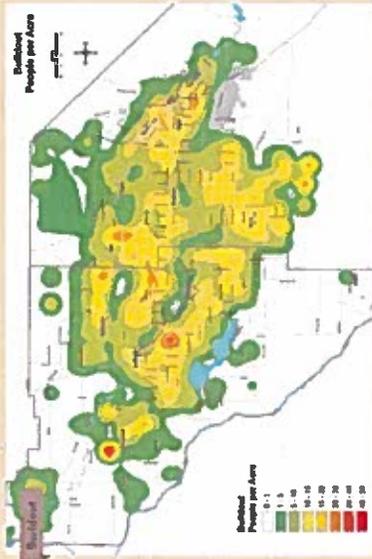
## Employment Density Maps



The 2010 maps above were the baseline conditions used in developing the *Communities in Motion 2040* Vision. These maps are based on the 2010 census tracts and 2010 Idaho Department of Labor employment data.



The above maps show the projected densities when the *Communities in Motion 2040* Vision is implemented.



Buildout is the quantification of land use (employment + jobs). It includes COMPASS as a consider long-range scenario. Employment used is not constrained by the 2040 population forecast outpiped by the COMPASS Buildout. It is an official forecast for air quality conformity.

## Norm Holm

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**From:** Eddy Thiel <eddy@nampahighway1.com>  
**Sent:** Tuesday, June 28, 2016 9:03 AM  
**To:** Norm Holm  
**Subject:** CMA 029-2016, ZMA 016-2016, PUD 037-2016

Good Morning Norman,

The Nampa Highway District #1 has no objection to the Comprehensive Plan Future Land Use Map Amendment from Public and Parks to Community Mixed Use, Rezone from AG to GB1, and Planned Unit Development Permit for Residential Uses at 1660 11<sup>th</sup> Ave No. for Doug Russell representing the Land Group Inc. For the Idaho Department of Health and Welfare as it is not within the Highway District's Jurisdiction.

If you have any questions or comments feel free to contact us.

Thank you,

Eddy

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Eddy Thiel  
ROW  
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## Shellie Lopez

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**From:** Neil Jones  
**Sent:** Wednesday, June 22, 2016 11:41 AM  
**To:** Shellie Lopez  
**Subject:** RE: Comprehensive Plan Future Land Use Map Amendment to Community Mixed Use, and Rezone from AG to GB1 ZMA 016-16 & CMA 029 16

Building Department has no conditions at this time.

### Neil Jones

Plans Examiner Supervisor

P: 208.468.5492 F: 208.468.4494

[Department of Building Safety](#), [Like us on Facebook](#)

**From:** Shellie Lopez  
**Sent:** Wednesday, June 15, 2016 9:59 AM  
**To:** Amanda Morse <morsea@cityofnampa.us>; Beth Ineck <ineckb@cityofnampa.us>; Brent Hoskins <hoskinsb@cityofnampa.us>; Carl Miller <CMiller@compassidaho.org>; Craig Tarter <tarterc@cityofnampa.us>; Daniel Badger <BadgerD@cityofnampa.us>; Don Barr <barrd@cityofnampa.us>; Eric Skoglund <skoglundl@cityofnampa.us>; Jennifer Yost <yostj@cityofnampa.us>; Jim Brooks <brooks@cityofnampa.us>; Michael Fuss <fussm@cityofnampa.us>; Neil Jones <jonesn@cityofnampa.us>; Patrick Sullivan <sullivanw@cityofnampa.us>; Ray Rice <ricer@cityofnampa.us>; Robin Collins <collinsrr@cityofnampa.us>; Soyla Reyna <reynas@cityofnampa.us>; Sylvia Mackrill <mackrill@cityofnampa.us>; Tina Fuller <tfuller@compassidaho.org>; Tom Laws <tlaws@compassidaho.org>; Vickie Holbrook <holbrookv@cityofnampa.us>  
**Subject:** Comprehensive Plan Future Land Use Map Amendment to Community Mixed Use, and Rezone from AG to GB1 ZMA 016-16 & CMA 029 16

Good Morning! ☺

### ZMA 016-16 & CMA 029 16:

Doug Russell of the Land Group, representing the State of Idaho Department of Health and Welfare, has requested a Comprehensive Plan Future Land Use Map Amendment from Public and Parks to Community Mixed Use, and Rezone from AG (Agricultural) to GB1 (Gateway Business 1) for a 615.6 acre parcel of land located in Sections 11, 12, 13, and 14, T3N, R2W, BM (Canyon County parcels R31440, R310550, R31070, R310710, R31143, R310820, R310830), including 1660 11<sup>TH</sup> Ave. N., Nampa.

The proposed Conceptual Master Plan includes the following:

- Commercial Office – 133 acres with +/- 1.9 million square feet of building facilities
- Mixed Use/ Retail/ Restaurant – 22.9 acres with +/- 245,650 square feet of building facilities
- Multi-Family Residential – 15.8 acres with 200 units and a recreation facility
- Single Family Residential – 63 acres with 258 units
- Retirement/ Assisted Living – 14 acres, 160 independent living units and assisted living units
- 3 Hotels
- Transit Center
- Redesigned 18 hole golf course
- Job Corps facilities to remain - +/- 191,000 square feet

## Christopher Daly

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**From:** Bill Hartman <bill\_hartman@hotmail.com>  
**Sent:** Wednesday, July 06, 2016 5:10 PM  
**To:** Christopher Daly  
**Subject:** ZMA016-2016 proposal from Health and Welfare

I am opposed to The Land Group request for a zoning change and modification of the comprehensive plan. The estimated income to the State of Idaho and the City of Nampa is just that, an estimate, who knows where they got those figures from and how valid they are. I must admit that my initial interest in this was because of the threat to our Municipal Golf Courses which I feel are a great attraction for the City of Nampa especially to attract quality businesses with above average paying jobs. While there is mention of a golf course in the proposal there is absolutely no guarantee it will come to pass and it will not be a quality affordable Municipal Course such as we have now. It is absolutely ridiculous that a city the size of Nampa would be without a Municipal Course. Our courses currently provide great recreation opportunities for adults, senior citizens, and many activities for our youth from Universities to High Schools clear down to 5 and 6 year olds, we don't want to lose that. Additionally as this has progressed I have now become very concerned that this proposal if allowed will be an environmental disaster as well as create an infrastructure nightmare that the costs of which will be enormous and probably be borne by the citizens of Nampa. The increased costs for the additional police and fire protection we don't even know what that will amount to, in addition to that the traffic mess that already exists will multiply, Winco is being built near Garrity Blvd when that is completed the traffic will be worse. I84 is always a complete mess from about 3 to 6 Monday through Friday and don't forget the traffic being created by Western Idaho Community College. I really feel that this is really bad idea for the reasons I have already stated and numerous others other people will bring up, and request that the rezone request and the comprehensive modification request be denied. Thank you for serving on this Planning and Zoning Commission.

Sent from [Mail](#) for Windows 10